

FIGURE VI.D: Conceptual Grading Plan
(West of Twin Oaks Valley Road)

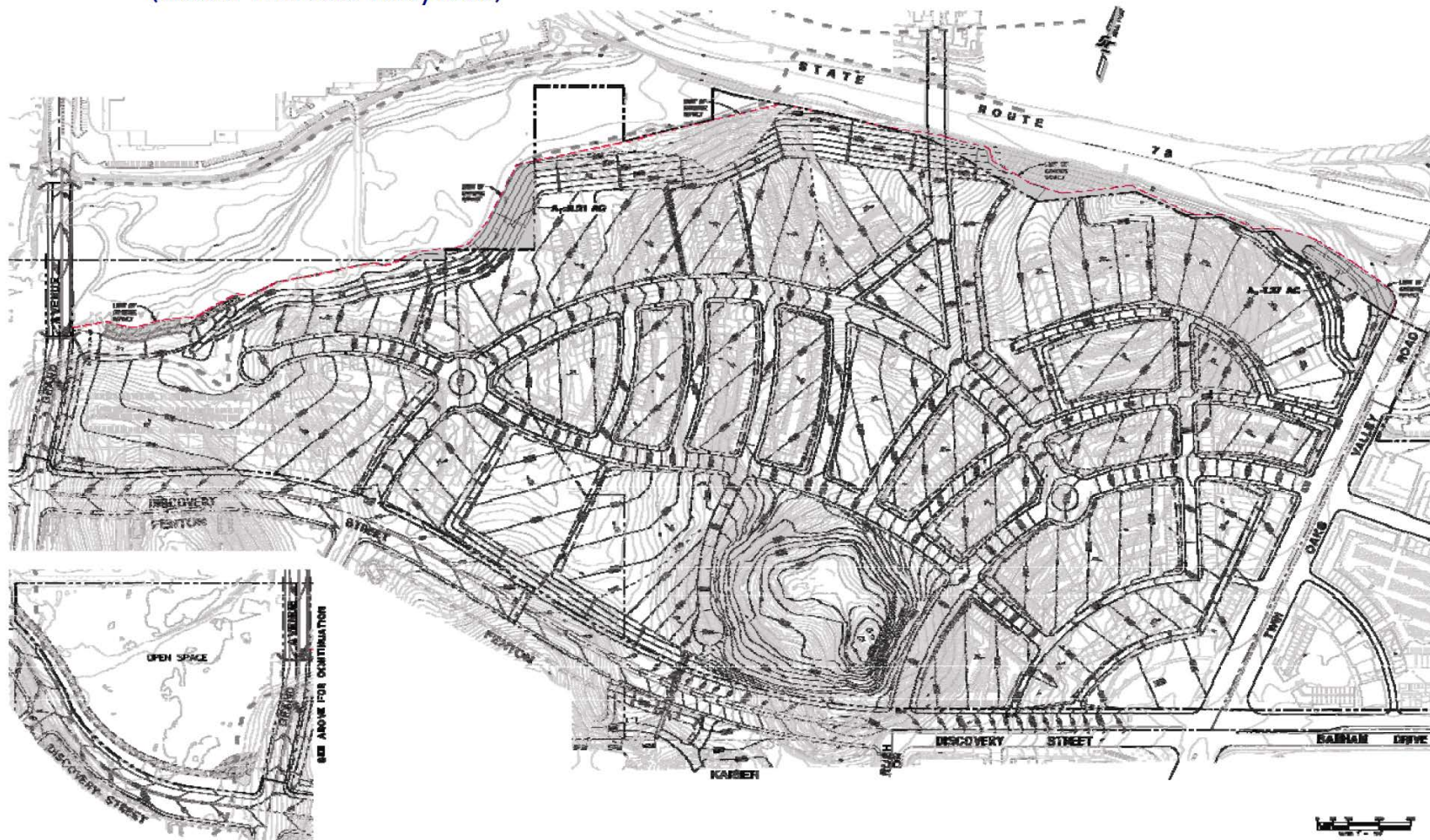
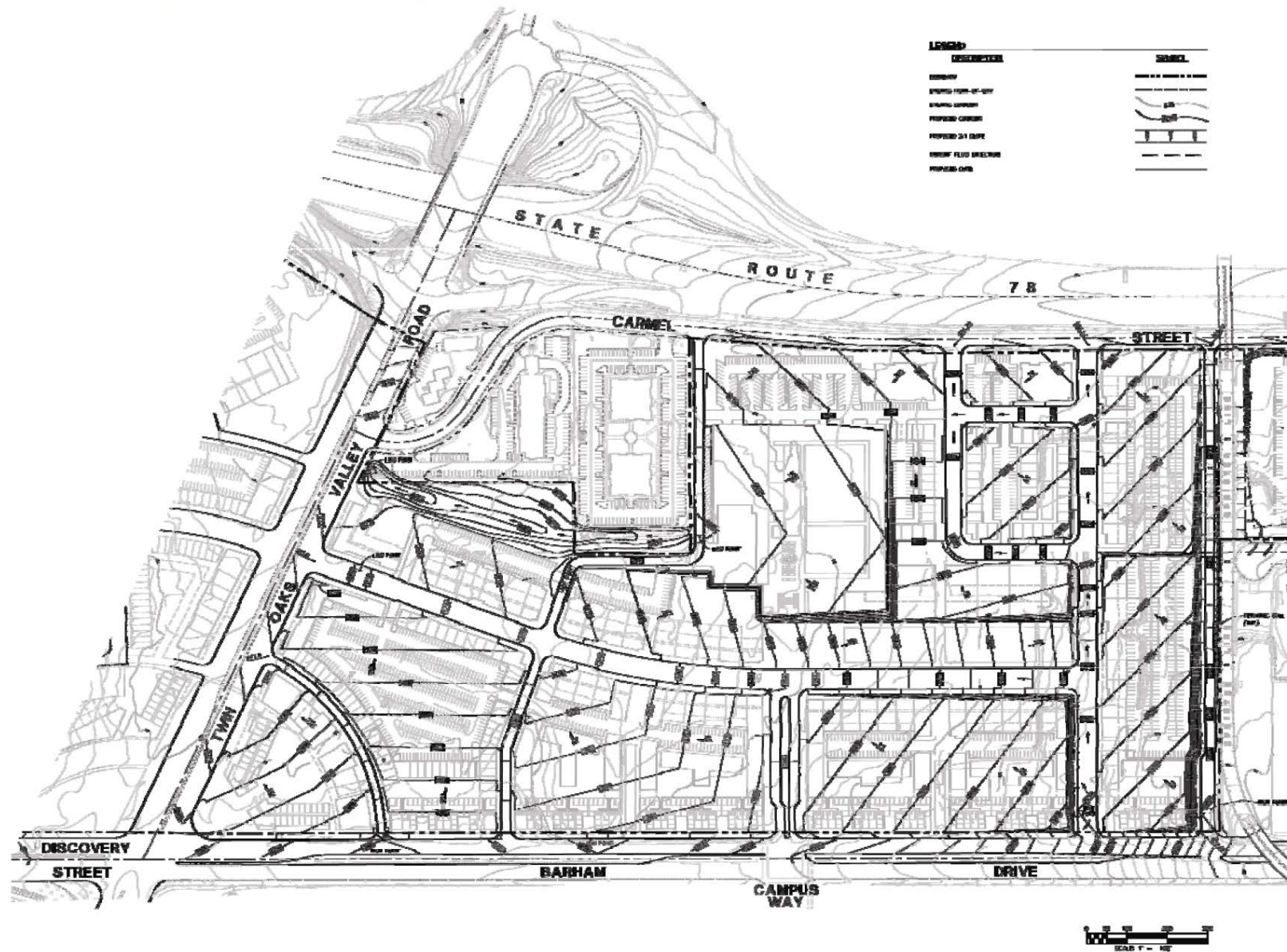


FIGURE VI.D: Conceptual Grading Plan
(East of Twin Oaks Valley Road)



VI.3 Block Standards

Intent

The *University District* is envisioned to be developed with an interconnected network of streets. The street network is intended to:

- ❑ Promote walking and alternative modes of transportation within the District by creating a pattern of relatively small blocks that are defined by pedestrian-friendly streets.
- ❑ Provide convenient access to California State University at San Marcos (CSUSM), the adjacent Sprinter Line Station, and adjacent neighborhoods.
- ❑ Provide access and connections to the parks, open spaces, and civic uses within the District.

Block Standards

As described in the Modifications sub-section of Section VI.2 (Regulating Plans), the Planning Director may approve deviations from the street and block network (as illustrated on the Regulating Plans) if the street network complies with the block standards provided in this section. All modifications shall comply with the following standards:

- ❑ Except as otherwise provided, block perimeters shall not exceed 1,600 linear feet. Block perimeter shall be measured by adding the length of all external property lines that are located adjacent to a street or pedestrian paseo. On the east side of Twin Oaks Valley Road, the block perimeters may be increased to 2,400 linear feet to allow for the construction of a public parking structure that is lined with mixed-use buildings.
- ❑ All new streets that intersect with Twin Oaks Valley Road, Barham Drive, and Discovery Street shall have centerline offsets of at least 200 feet between existing and planned intersections.

- ❑ Any single block face longer than 550 linear feet must include access to a publicly dedicated sidewalk or pedestrian passage that connects to the opposite block face. The sidewalk or pedestrian passage shall be at least 8 feet wide and may go under or through buildings. This standard does not apply to blocks that contain a mixed-use building with an above ground public parking structure ~~or an anchor retail store.~~
- ❑ If the *University District* is subdivided in phases by different property owners/developers, each subdivision shall be graded and designed with street stubs to adjoining areas within the District to accommodate future street connectivity. All new streets must be publicly dedicated. Private streets ~~and/or gated streets are prohibited.~~ may be permitted with a Site Development Permit (SDP). Gated streets may be permitted on private streets with a Site Development Permit (SDP).
- ❑ Dead-end streets are not permitted except where physical conditions, such as State Route 78, open space areas, or unique topographic features, provide no practical connection alternatives. If a street terminates near an open space area, a visual corridor between the street and the open space area shall be required. The visual corridor shall be at least 50 feet wide.

The blocks described below are exempt from the above-referenced block standards:

- ❑ Any block that includes adjacent properties that are not located within the *University District* project boundary.
- ❑ Any block that is adjacent to the Sprinter Line.
- ❑ Any block that is located adjacent to the open space along State Route 78 and San Marcos Creek (including the East Creek Park, the West Creek Park, and the Creek Trail).
- ❑ Blocks that would be occupied by the public school site, the Knoll Park, and the Wetland Trail and Open Space.
- ❑ Any block that is to be developed with a mixed-use building type with a public parking structure ~~or an anchor retail store.~~

VI.4 Street Type Standards

This section addresses the comprehensive design of all public streets within the *University District*. It includes design standards for the allowed street types, sidewalk and crosswalk pavement, streetlights, and street furniture. Additional landscaping standards for streets are provided in Section VI.9 (Landscaping Standards).

Intent

The *University District* is envisioned to be developed with streets that facilitate the movement of people through a variety of modes of transportation, including walking, bicycling, transit, and automobiles. Certain streets will be designed as attractive public spaces that are enhanced with wide tree-lined sidewalks and attractive streetscape furniture. Certain streets will also be designed with Low-Impact Development (LID) facilities to address storm water management. LIDs that will be used within street right-of-ways include, but are not limited to:

- ❑ Underground Storm Water Storage Cisterns: Cisterns are closed below ground storage vessels that are located beneath the sidewalk or street. Cisterns capture run-off and/or overflow from streets, roofs, impervious surfaces, and other LID features.
- ❑ Tree-Grate Flow-Through Planters: Tree grate flow-through planters are planted bio-retention areas located between the sidewalk and the curb and gutter. Tree grate flow through planters capture storm water run-off from streets, roofs, impervious surfaces, and other LID features.
- ❑ Flow-Through Planters: Flow-Through Planters are planted bio-swales located within medians or along the edges of streets. The plant root systems within the flow through planters filter run-off before it percolates into the ground.
- ❑ Other measures acceptable to the City in order to meet the intent of LID design.



Streets are envisioned to include consistent landscaping and landscape patterns to identify streets and create a sense of place. Streets along public spaces (such as a

park or plaza), at a major intersection, or other unique feature may deviate from the palette for that street to signify the feature; however, use of similar species, landscape patterns, and amenities is envisioned. Specific palettes for each street shall be prepared prior to approval of engineering plans. The Planning Division Director may approve alternative landscape plans with a Site Development Permit.

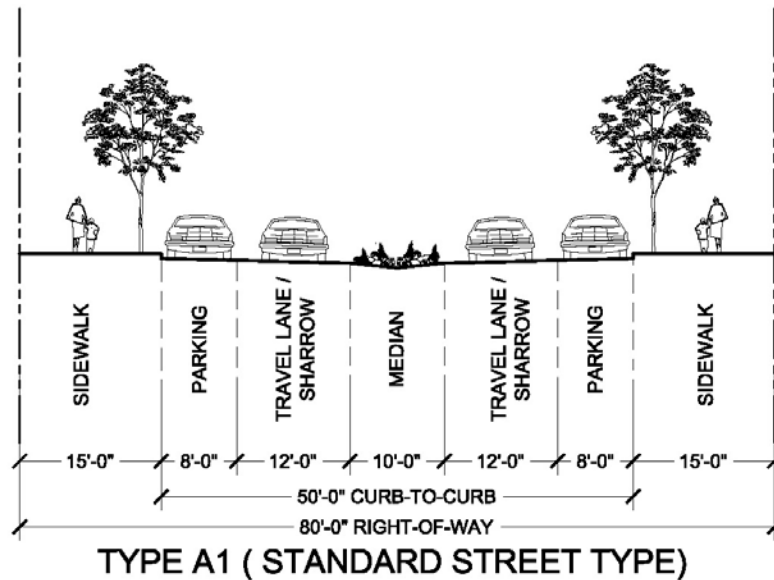
Allowed Street Types

A variety of street types are allowed within the *University District*. Figure VI.A: Street Type Regulating Plan shows the locations for the various street types. Design standards for each street type are provided on the following pages.

For each street type, there are standards that address:

- ☐ Design Speed
- ☐ Right-of-Way Width
- ☐ Curb-to-Curb Width
- ☐ Parking Lane Width
- ☐ Bike Lane ~~Width~~ (The use of either bike lanes or sharrows shall be at the discretion of the City Engineer and appropriate regulatory guidelines)
- ☐ Travel Lane Width
- ☐ Drainage Type
- ☐ Width of Sidewalks
- ☐ Other Unique Features
- ☐ Trees ~~Species~~ and Spacing

Street Type A-1 (~~Spine~~Standard Street Type)



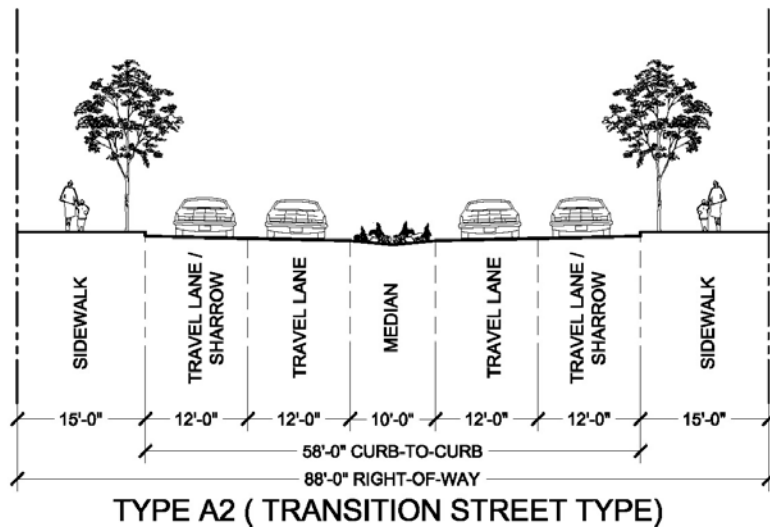
Standards:

Design Speed:	25 mph
Right-of-Way Width:	80'
Curb-to-Curb Width:	50'
Parking Lane Width:	8'
Bike Lane Width:	5' <u>Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)</u>
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; <u>LID in Some Locations Center Median</u>
Width of Sidewalks:	15'
Other Unique Features:	Not Applicable
Trees Species and Spacing:	Tipuana tipu <u>Cassia leptophylla</u> Spaced at 30' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u>

Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Street Type A-2 (~~Spine~~Transition Street ~~at Plaza~~Type)



Standards:

Design Speed:	25 mph
Right-of-Way Width:	85'-9" 88'
Curb-to-Curb Width:	50'-6" 58'
Parking Lane Width:	8' Not Applicable8'
Bike Lane Width :	5'-Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations Center Median
Width of Sidewalks:	20' and 15'
Other Unique Features:	Not Applicable Trees Planted in 5' x 8' Tree Grates with Structural Soil
Trees Species and Spacing:	Tipuana tipu <u>Cassia leptophylla</u> Spaced at 30' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u>

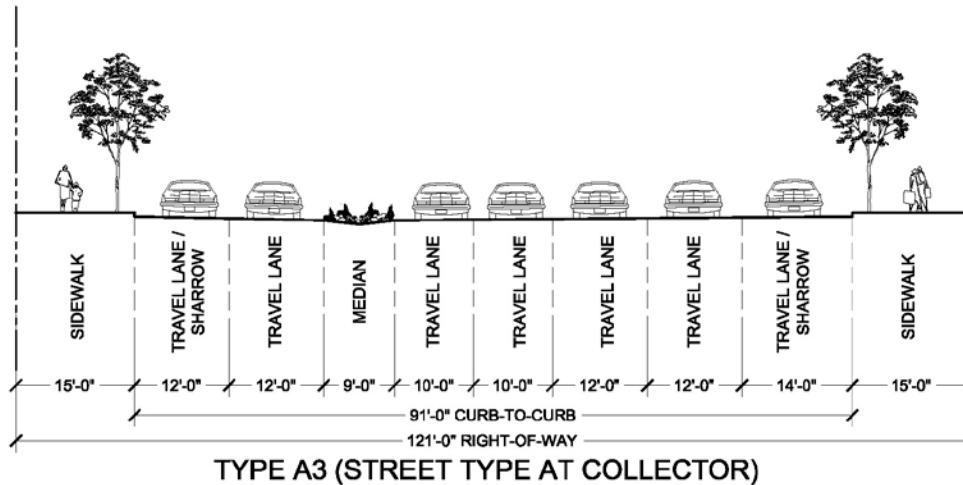
Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

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Street Type A-3 (~~Spine-Street~~ Type at Collector)



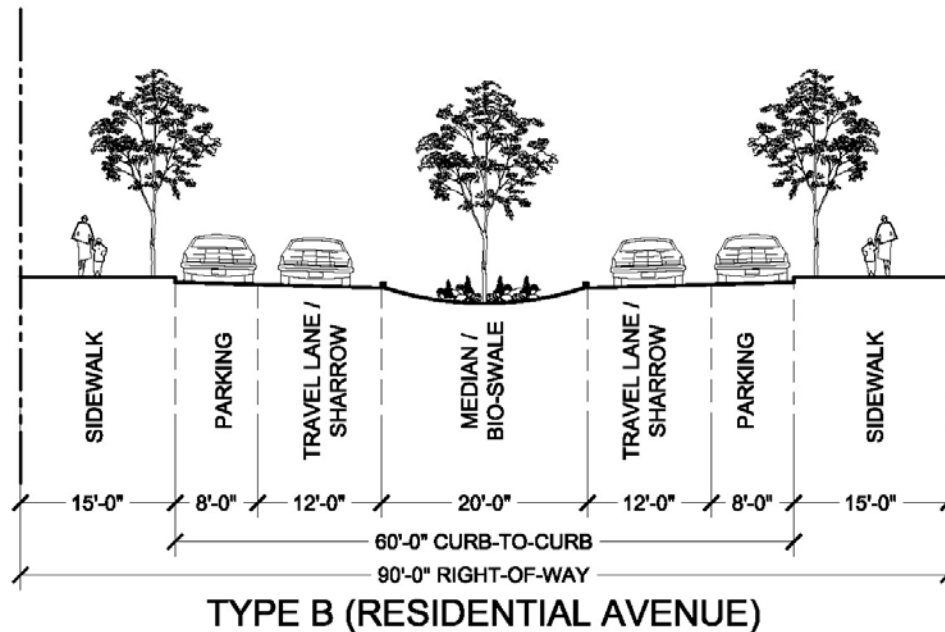
Standards:

Design Speed:	25 mph
Right-of-Way Width:	86'-12" <u>113'</u>
Curb-to-Curb Width:	56'-9" <u>91'</u>
Parking Lane Width:	Not Applicable
Bike Lane Width:	<u>Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)</u> <u>5'</u>
Travel Lane Width:	11' to 12' <u>10' to 14'</u> <u>12', 14'</u>
Drainage Type:	<u>Curb, Gutter, and Storm Drain; LID in Some Locations</u> <u>Center Median</u>
Width of Sidewalks:	15'
Other Unique Features:	<u>Trees Planted in 5' x 8' Tree Grates with Structural Soil</u> <u>Not Applicable</u>
Trees Species and Spacing:	<u>Quercus agrifolia and Tipuana tipu - Cassia leptophylla</u> Spaced at 30' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u>

Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

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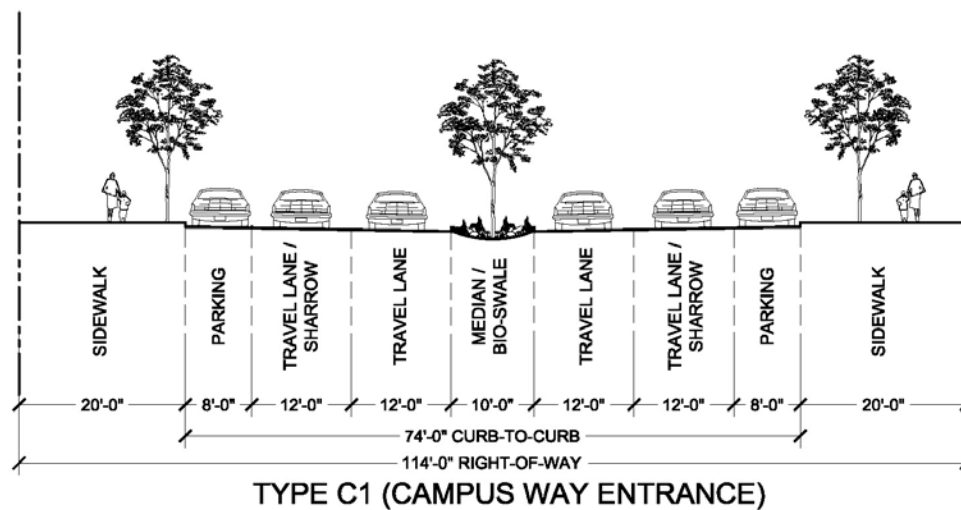
Street Type B (Residential Avenue)**Standards:**

Design Speed:	25 mph
Right-of-Way Width:	96'-90'
Curb-to-Curb Width:	25' (Each Side of Median) 60'
Parking Lane Width:	8'
Bike Lane Width:	5' Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations Center Median / Bioswale
Width of Sidewalks:	15'
Other Unique Features:	±6' Median Swale; Trees Planted in 5' x 8' Tree Grates with Structural Soil Not Applicable
Tree Species and Spacing:	Species Vary: <i>Platanus racemosa</i> and <i>Populus fremontii</i> Spaced at 25' Maximum on Center; <i>Alnus rhombifolia</i> Spaced at 20' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u>

Notes:

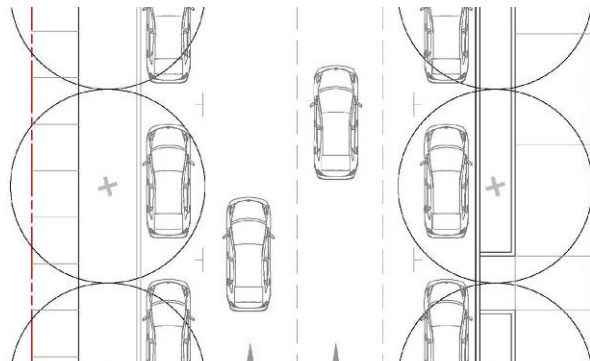
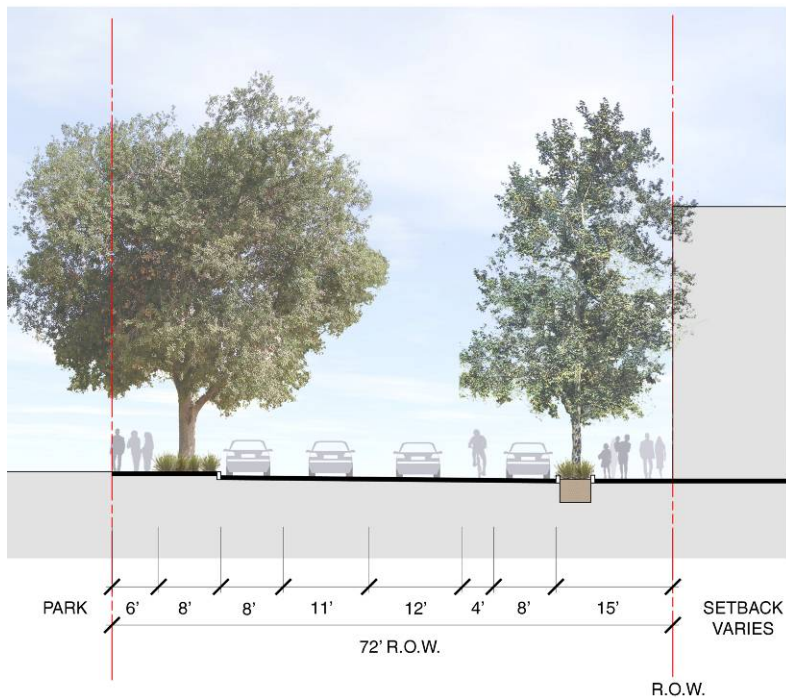
The spacing of street trees may be modified where curb-cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

Street Type C₁ (Campus Way Entrance)



Standards:

Design Speed:	25 mph
Right-of-Way Width:	110' <u>114'</u>
Curb-to-Curb Width:	74' <u>74' (Each Side of Median)</u>
Parking Lane Width:	Not Applicable <u>8'</u>
Bike Lane Width:	Not Applicable <u>Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)</u>
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations <u>Center Median</u>
Width of Sidewalks:	20'
Other Unique Features:	22' Median Swale; Trees Planted in 5' x 8' Tree Grates with Structural Soil <u>Not Applicable</u>
Tree Species and Spacing:	<p>Accent Tree: <i>Syagrus romanzoffianum</i> Spaced at 15' Maximum on Center;</p> <p>Street Tree: <i>Tipuana tipu</i> <i>Callistemon viminalis</i> Spaced at 30' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u></p>

Street Type D-1 (One-Way Parkway)

Form-

Notes:

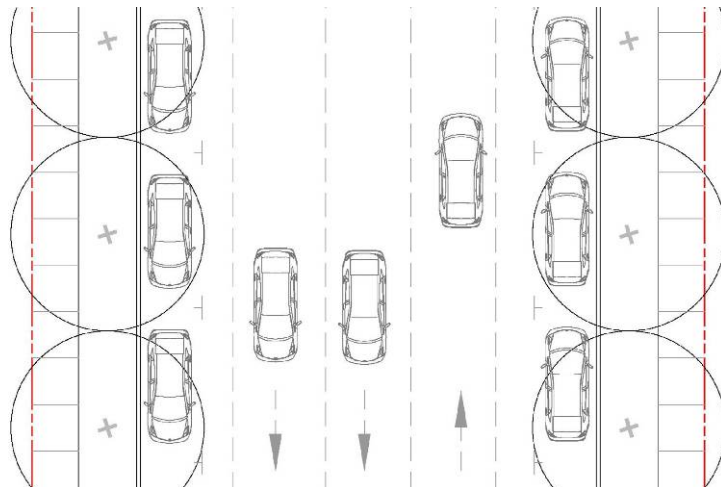
The spacing of street trees may be modified where curb cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Standards:

Design Speed:	25 mph
Right-of-Way Width:	72'
Curb to Curb Width:	43'
Parking Lane Width:	8'
Bike Lane Width:	4'
Travel Lane Width:	11' to 12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	6' and 15'
Other Unique Features:	Flow-Through Planters; 8' Parkway
Tree Species and Spacing:	Species Vary: <i>Quercus agrifolia</i> Spaced at 30' Maximum on Center; <i>Platanus racemosa</i> Spaced at 25' Maximum on Center

Notes:

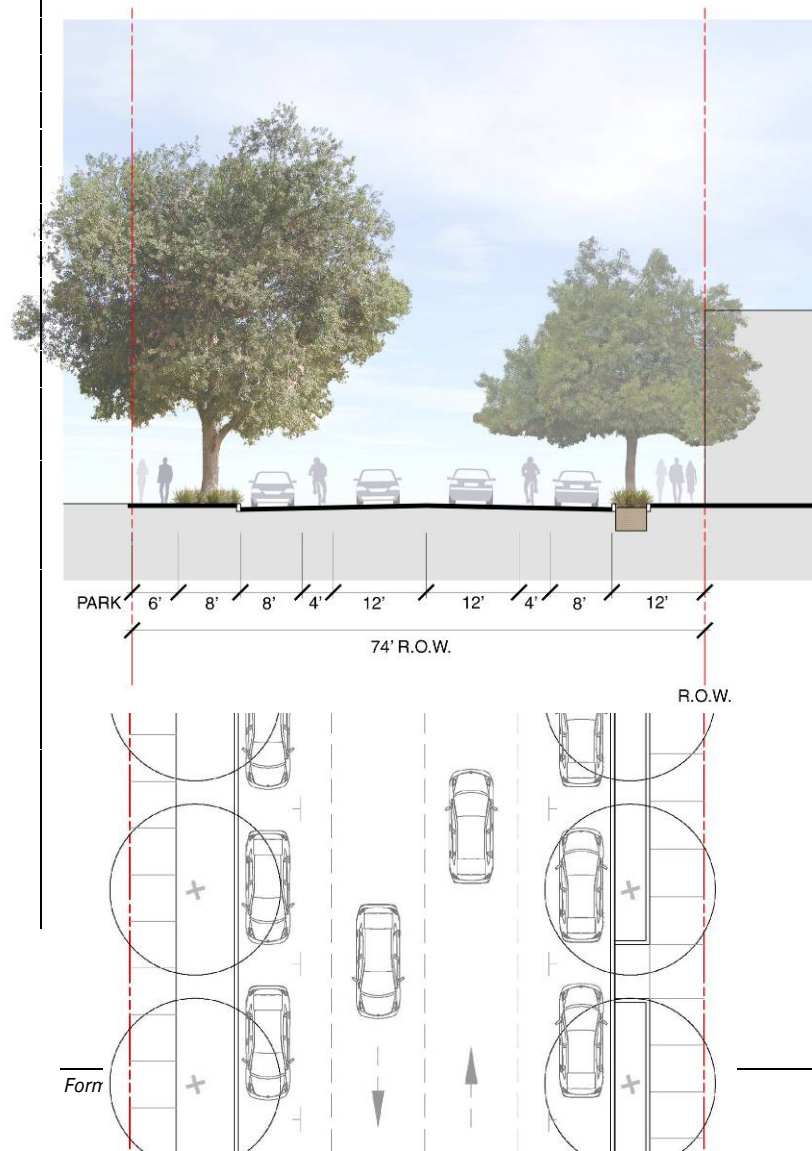
The spacing of street trees may be modified where curb cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Street Type D-2 (Two-Way Parkway)**Standards:**

Design Speed:	25 mph
Right-of-Way Width:	87'
Curb-to-Curb Width:	59'
Parking Lane Width:	8'
Bike Lane Width:	4'
Travel Lane Width:	11' and 12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	6'
Other Unique Features:	8' Parkway
Tree Species and Spacing:	<i>Quercus agrifolia</i> Spaced at 30'

Notes:

The spacing of street trees may be modified where curb cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Street Type E-1 (Two-Lane Collector Street at Park/School)**Standards:**

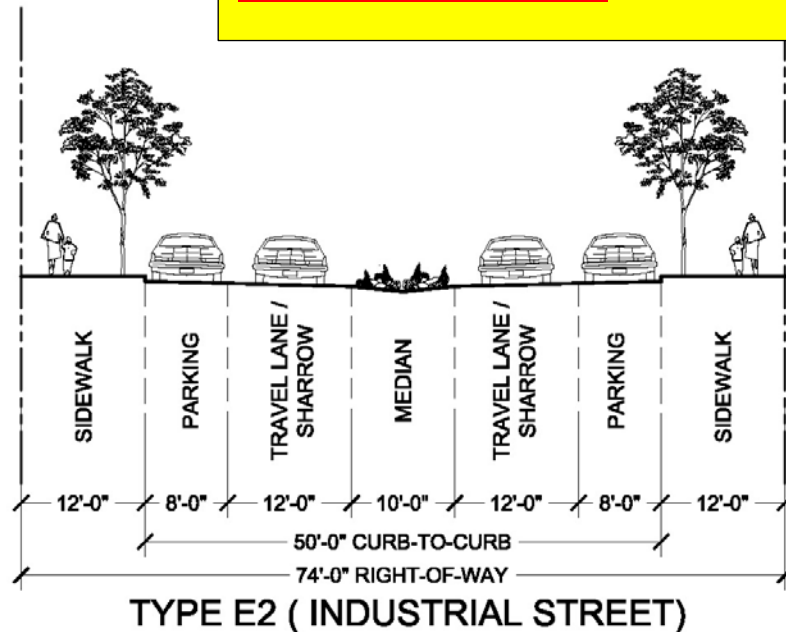
Design Speed:	25 mph
Right-of-Way Width:	74'
Curb-to-Curb Width:	48'
Parking Lane Width:	8'
Bike Lane Width:	4'
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	6' and 12'
Other Unique Features:	Flow Through Planters; 8' Parkway
Tree Species and Spacing:	<i>Arbutus 'Marina'</i> and <i>Rhus lancea</i> Spaced at 25' Maximum on Center

Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Street Type E-2-2 (Two-Lane Collector/Industrial Street)

Street section figure to be updated to reflect dimensions in table



Standards:

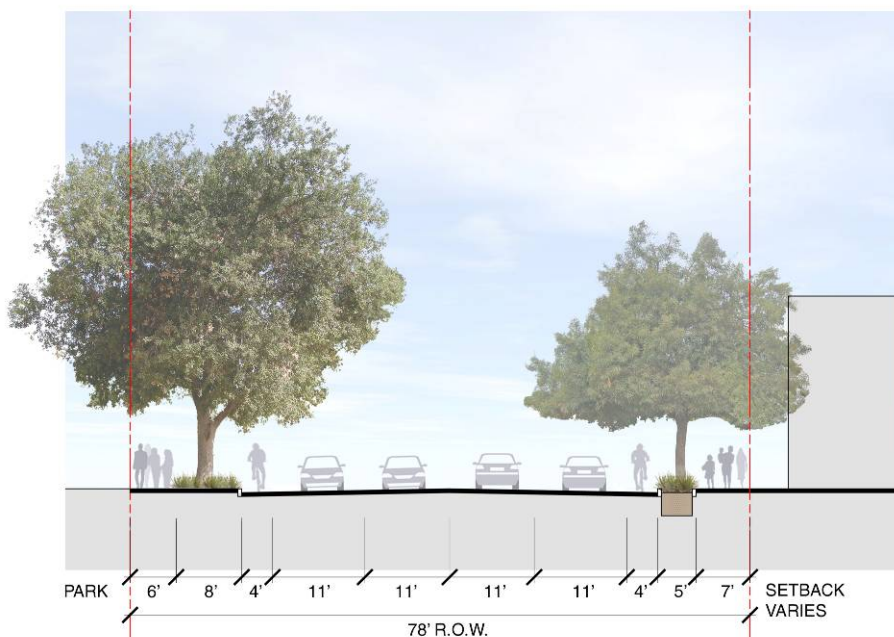
Design Speed:	25 mph
Right-of-Way Width:	74'-70'
Curb-to-Curb Width:	50'-84'-0'
Parking Lane Width:	8'
Bike Lane Width:	<u>Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)4'</u>
Travel Lane Width:	12'
Drainage Type:	<u>Curb, Gutter, and Storm Drain; LID in Some LocationsCenter Median</u>
Width of Sidewalks:	12'-15'
Other Unique Features:	<u>Trees Planted in 5' x 8' Tree Grates with Structural Soil</u>
Tree Species and Spacing:	<u>Arbutus 'Marina' and Rhus lancea Spaced at 25' Maximum on Center. May be modified through Site Development Permit or by Director's Permit</u>

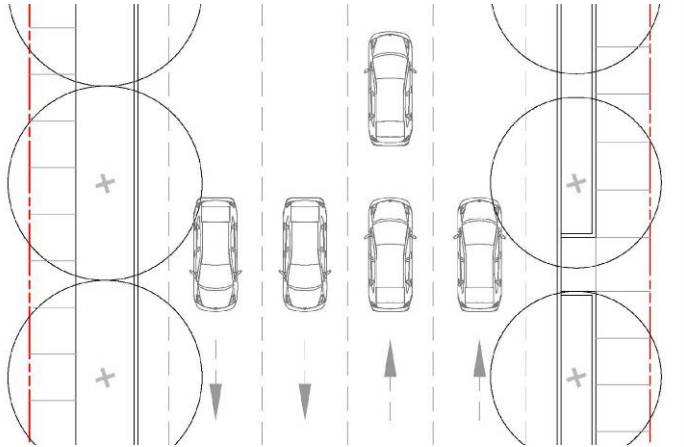
Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Standards:	
Design Speed:	25 mph
Right-of-Way Width:	78'
Curb-to-Curb Width:	52'
Parking Lane Width:	Not Applicable
Bike Lane Width:	4'
Travel Lane Width:	11'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	6' and 12' (Setback Varies)
Other Unique Features	Flow-Through Planters; 8'

Street Type E-3 (Four-Lane Collector)



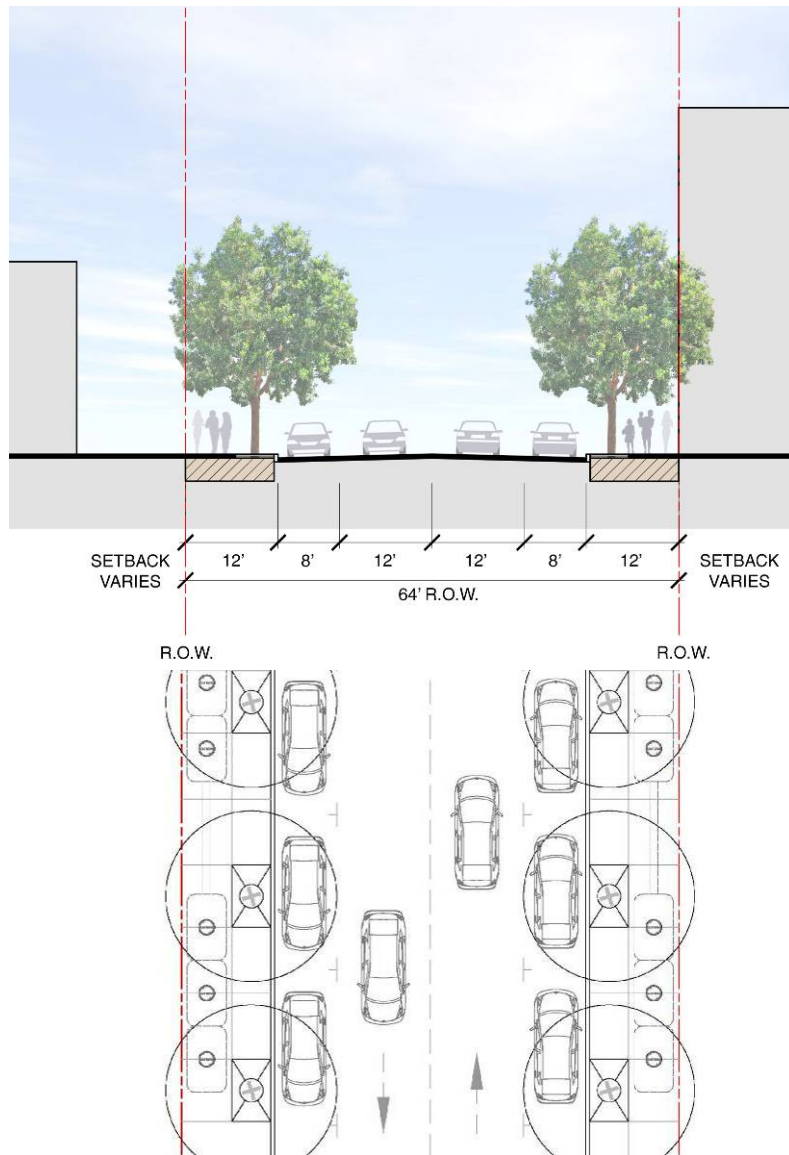


Parkway

Tree Species and Spacing: Species Vary:
Quercus agrifolia Spaced at 30'
 Maximum on Center; *Rhus lancea*
 Spaced at 25' Maximum on
 Center

Notes:

The spacing of street trees may be modified where curb cuts
 are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Street Type F (Mixed-Use Street)**Standards:**

Design Speed: 25 mph

Right-of-Way Width: 64'

Curb-to-Curb Width: 40'

Parking Lane Width: 8'

Bike Lane Width: Not Applicable

Travel Lane Width: 12'

Drainage Type: Curb, Gutter, and Storm Drain, LID in Some Locations

Width of Sidewalks: 12'

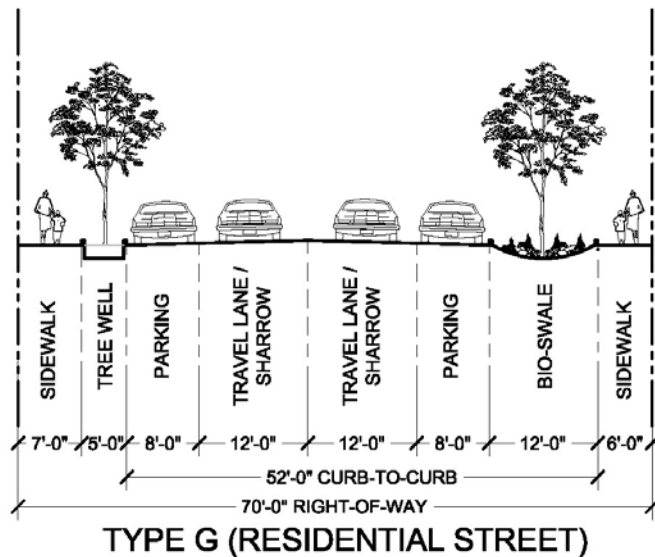
Other Unique Features: Trees Planted in 5' x 8' Tree Grates with Structural Soil

Tree Species and Spacing: *Arbutus* 'Marina' Spaced at 25' Maximum on Center**Notes:**

The spacing of street trees may be modified where curb cuts are required for driveway access;

See Figure VI.E: LID in Public R.O.W.;

See Figure VI.F: Street Tree Diagram

Street Type G (Residential Street)**Standards:**

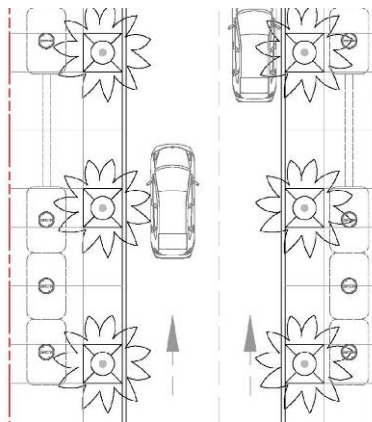
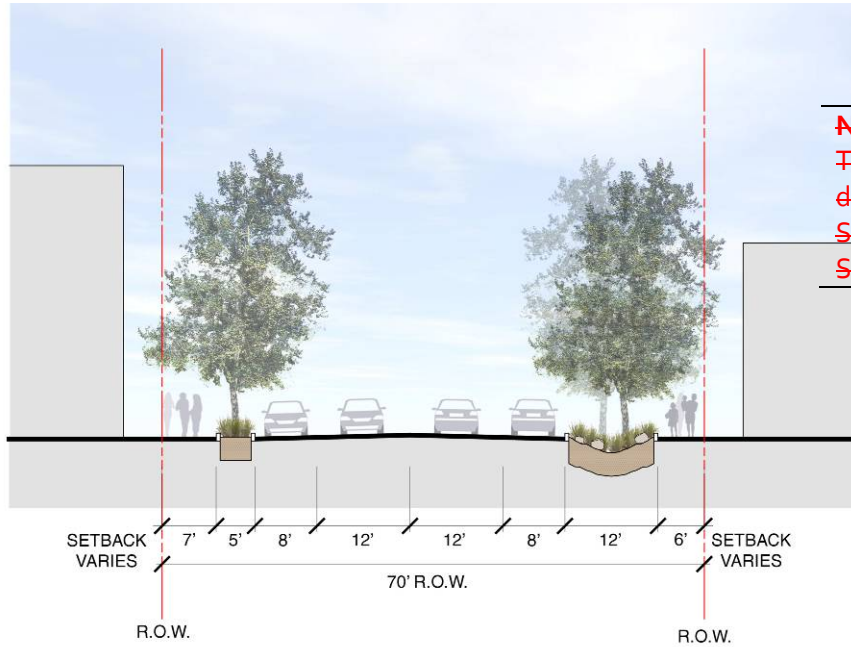
Design Speed:	25 30 mph
Right-of-Way Width:	48' 70'
Curb-to-Curb Width:	24' 52'
Parking Lane Width:	Not Applicable 8'
Bike Lane Width:	Sharrows (Per the discretion of the City Engineer and appropriate regulatory guidelines) Not Applicable
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations Flow-Through Planter
Width of Sidewalks:	6', 7' 12'
Other Unique Features:	Trees Planted in 5' x 8' Tree Grates with Structural Soil
Tree Species and Spacing:	<i>Platanus racemosa</i> and <i>Rhus lancea</i> Spaced at 25' Maximum on Center. May be modified through Site Development Permit or by Director's Permit

Notes:

The spacing of street trees may be modified where curb-cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram

Standards:	
Design Speed:	15 mph
Right-of-Way Width:	47'
Curb-to-Curb Width:	20'
Parking Lane Width:	Not Applicable
Bike Lane Width:	Not Applicable
Travel Lane Width:	8' and 12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	15' and 12'
Other Unique Features:	Passenger Loading Lane: 8'; Trees Planted with 5' x 5' Tree Grates in

Street Type H (One-Way Access Street)



Tree Species and Spacing:

Structural Soil

Species Varies:

Syagrus romanzoffianum Spaced at 20'
Maximum on Center, *Rhus lancea* Spaced
at 25' Maximum on Center

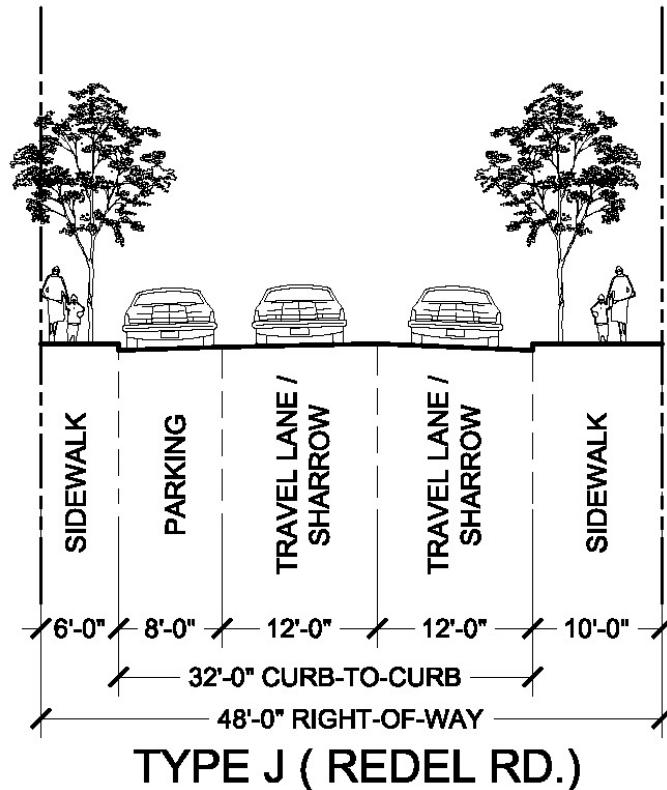
Notes:

The spacing of street trees may be modified where curb cuts are required for driveway access;

See Figure VI.E: LID in Public R.O.W.;

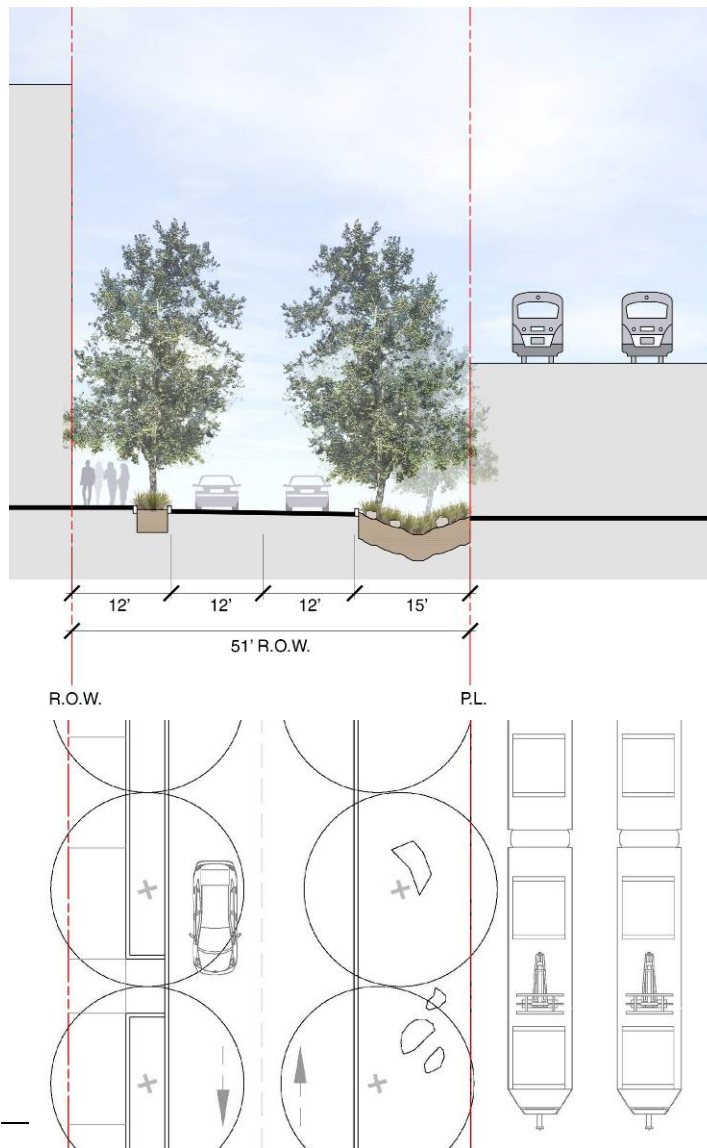
See Figure VI.F: Street Tree Diagram

Street Type J (~~Redel Road~~ Two-Way Access Street)



Standards:

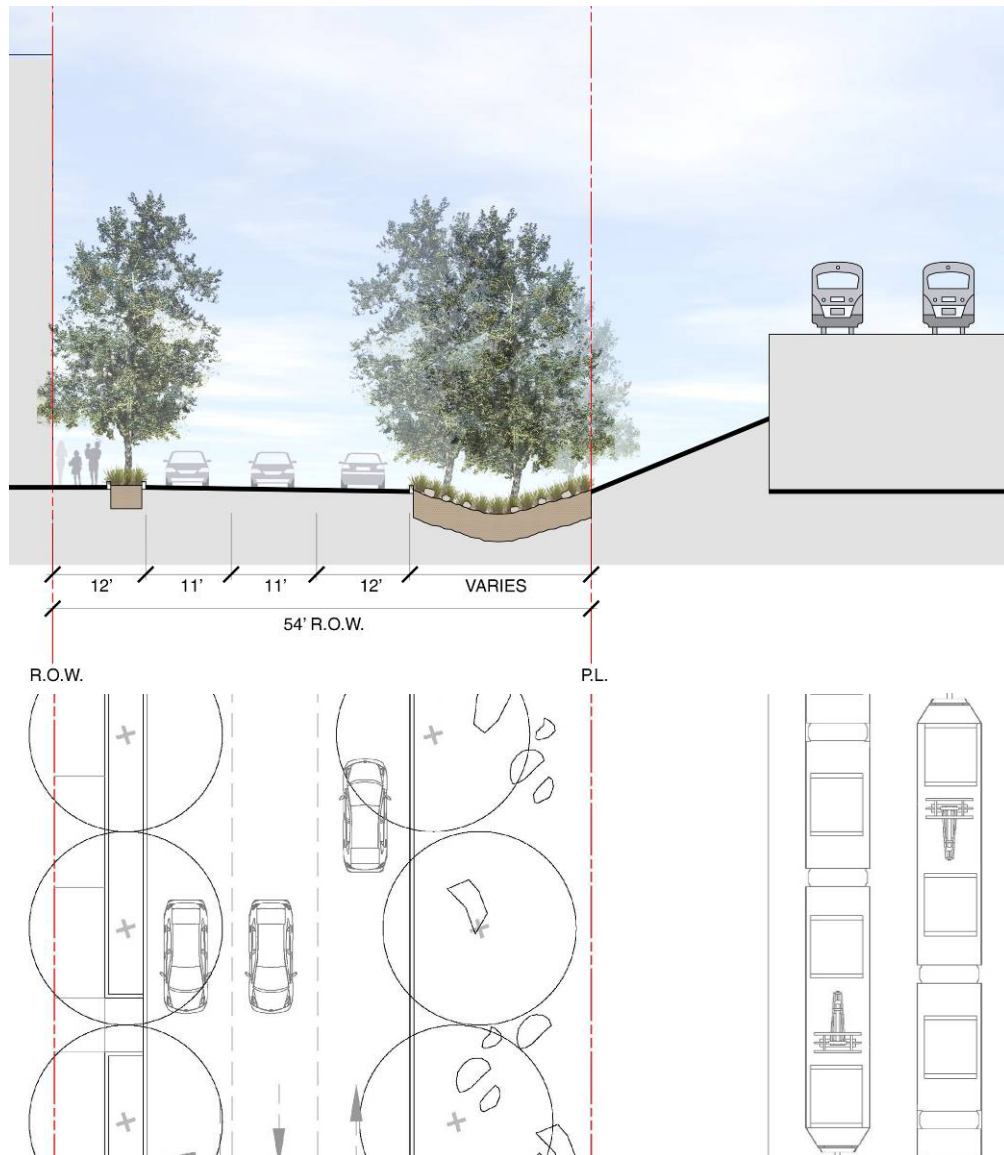
Design Speed:	25 0 mph
Right-of-Way Width:	48'
Curb-to-Curb Width:	24' 32'
Parking Lane Width:	Not Applicable 8'
Bike Lane Width:	Not Applicable Sharrow (Per the discretion of the City Engineer and appropriate regulatory guidelines)
Travel Lane Width:	12'
Drainage Type:	Curb and Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	6', 10' 12'
Other Unique Features:	Trees Planted in 5' x 8' Tree Grates with Structural Soil
Tree Species and Spacing:	Arbutus 'Marina', Rhus lancea, and Schinus molle Phoenix doctylifera Spaced at 25' Maximum on Center. May be modified through Site Development Permit or by Director's Permit

Street Type J-1 (Sprinter Line Access Street)**Standards:**

Design Speed:	20 mph
Right-of-Way Width:	51'
Curb-to-Curb Width:	24'
Parking Lane Width:	Not Applicable
Bike Lane Width:	Not Applicable
Travel Lane Width:	12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalk:	12' (One Side Only)
Other Unique Features	15' Bioswale; Flow Through Planters
Tree Species and Spacing:	<i>Alnus rhombifolia</i> , <i>Platanus racemosa</i> , and <i>Populus fremontii</i> Spaced at 25' Maximum on Center

Notes:

The spacing of street trees may be modified where curb cuts are required for driveway access;
 See Figure VI.E: LID in Public R.O.W.;
 See Figure VI.F: Street Tree Diagram



**Street Type J-2
(Sprinter Line Access
Street w/ Turn
Lane) Standards:**

Design Speed:	20 mph
Right-of-Way Width:	54'
Curb-to-Curb Width:	34'
Parking Lane Width:	Not Applicable
Bike Lane Width:	Not Applicable
Travel Lane Width:	11' to 12'
Drainage Type:	Curb, Gutter, and Storm Drain; LID in Some Locations
Width of Sidewalks:	12' (One Side Only)
Other Unique Features:	Bioswale; Flow-Through Planters
Tree Species and Spacing:	<i>Alnus rhombifolia</i> , <i>Platanus racemosa</i> , and <i>Populus fremontii</i> Spaced at 25' Maximum on Center

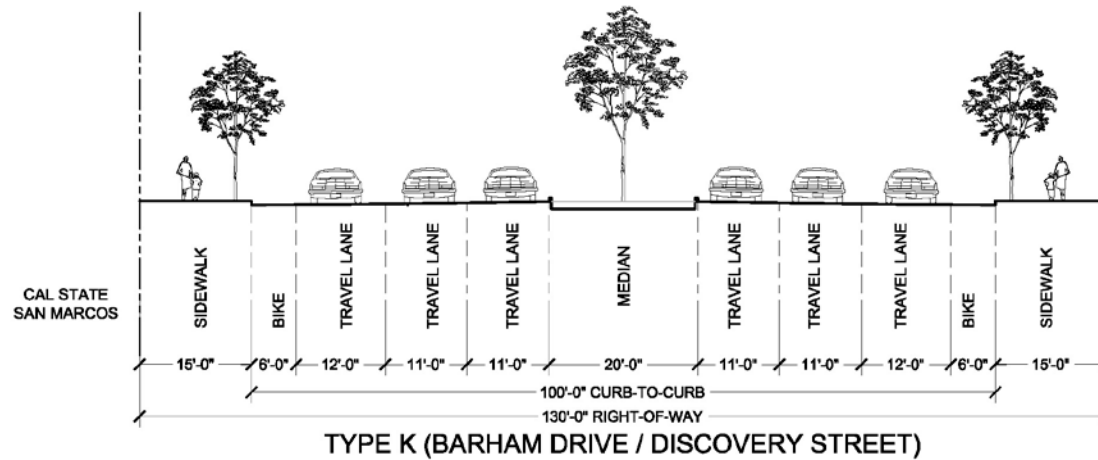
Notes:

The spacing of street trees may be modified where curb cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

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Street Type K (Barham Drive/Discovery Street)

Street section figure to be updated to reflect dimensions in table



Street section figure to be updated for Barham to include 15' sidewalk on north side, 5' bike lane, 3 11' lanes, 24' median, 3 11' lanes, 5' bike lane, and 13' sidewalk on the south side. Street Type K will become K-1 and the Barham section will be K-2

Standards:

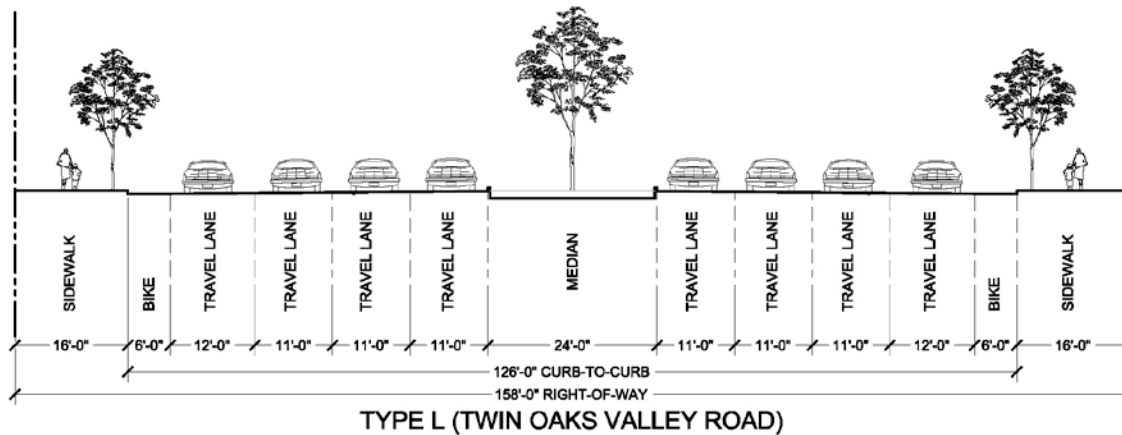
Design Speed:	45 mph
Right-of-Way Width:	130' 132'
Curb-to-Curb Width:	40' (Each Side of Median) 100'
Parking Lane Width:	Not Applicable
Bike Lane Width:	6'5' (Per the discretion of the City Engineer and appropriate regulatory guidelines)
Travel Lane Width:	11'-10" 12'
Drainage Type:	Curb and Gutter, and Storm Drain, LID in Some Locations
Width of Sidewalks:	15'16' and 10'
Other Unique Features	20' Median; Trees and Spacing to Match Existing
Tree Species and Spacing:	Median: <i>Pinus Pine</i> Spaced at 50' Maximum on Center; Street Tree: <i>Lagerstroemia</i> 'Muskogee' Spaced at 25' Maximum on Center. <u>May be modified through Site Development Permit or</u>

by Director's Permit

Notes:

The spacing of street trees may be modified where
curb-cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

Street Type L (Twin Oaks Valley Road)



Street section figure to be updated to reflect dimensions in table

Standards:

Design Speed:	45 mph
Right-of-Way Width:	158' <u>157'</u>
Curb-to-Curb Width:	126' <u>125'</u> (Each Side of Median)
Parking Lane Width:	Not Applicable
Bike Lane Width:	<u>6'-5"</u> (Per the discretion of the City Engineer and appropriate regulatory guidelines)
Travel Lane Width:	11'-10" <u>11'-12"</u>
Drainage Type:	<u>Curb, Gutter, and Storm Drain; LID in Some Locations</u>
Width of Sidewalks:	<u>16'</u> (Setback Varies)
Other Unique Features	<u>24' Median; Trees and Spacing to Match Existing</u>
Tree Species and Spacing:	Median: <i>Pinus Pinea</i> Spaced at 50' Maximum on Center; Street Tree: <i>Tipuana</i> <u><i>Cassia leptophylla</i></u> Spaced at 35' Maximum on Center. <u>May be modified through Site Development Permit or by Director's Permit</u>

Notes:

The spacing of street trees may be modified where

curb-cuts are required for driveway access;
See Figure VI.E: LID in Public R.O.W.;
See Figure VI.F: Street Tree Diagram

FIGURE VI.E: LID In Public R.O.W.

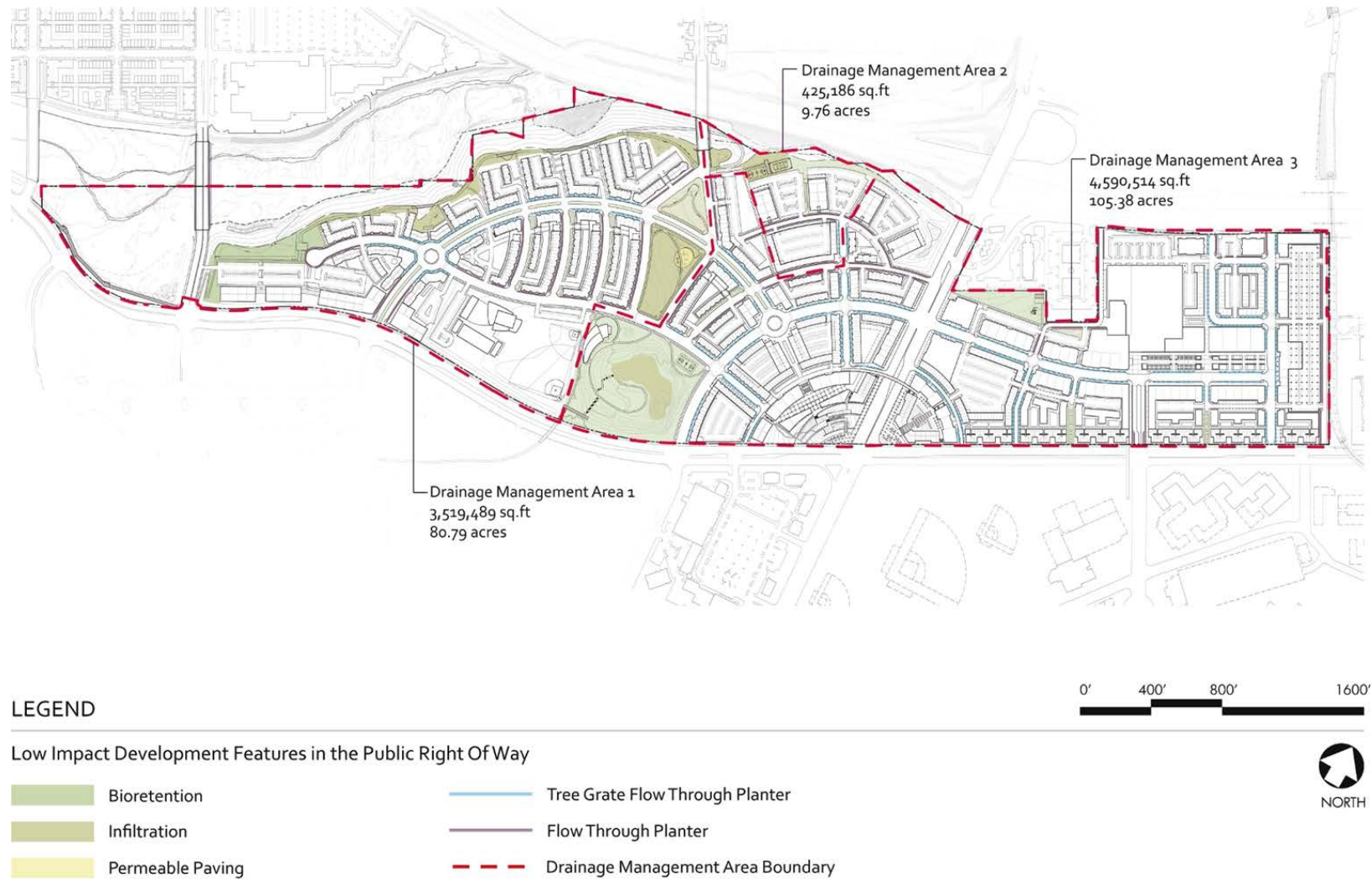


Figure VI.E: LID in Public R.O.W. (West)

Figure to be revised to make it more legible

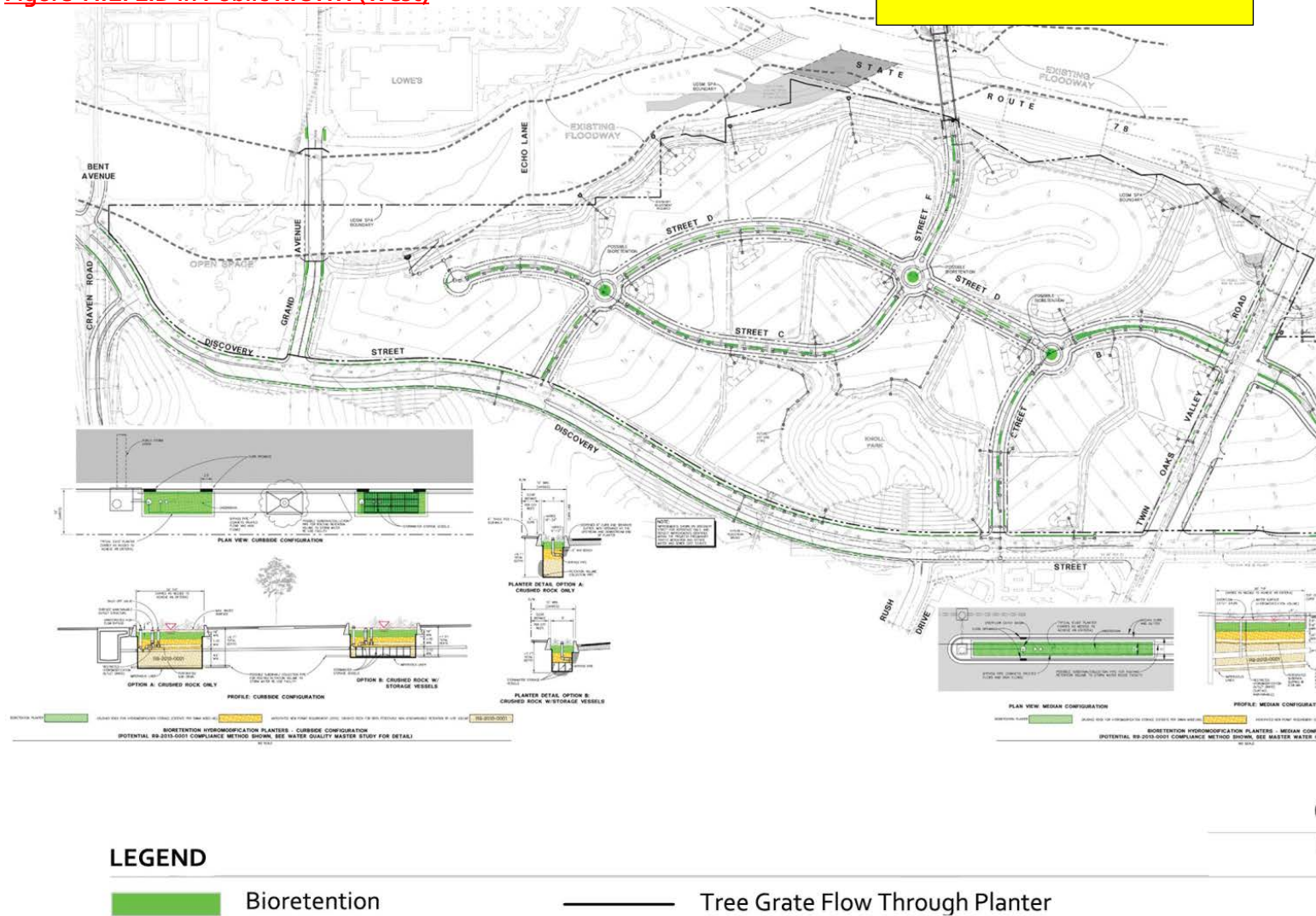
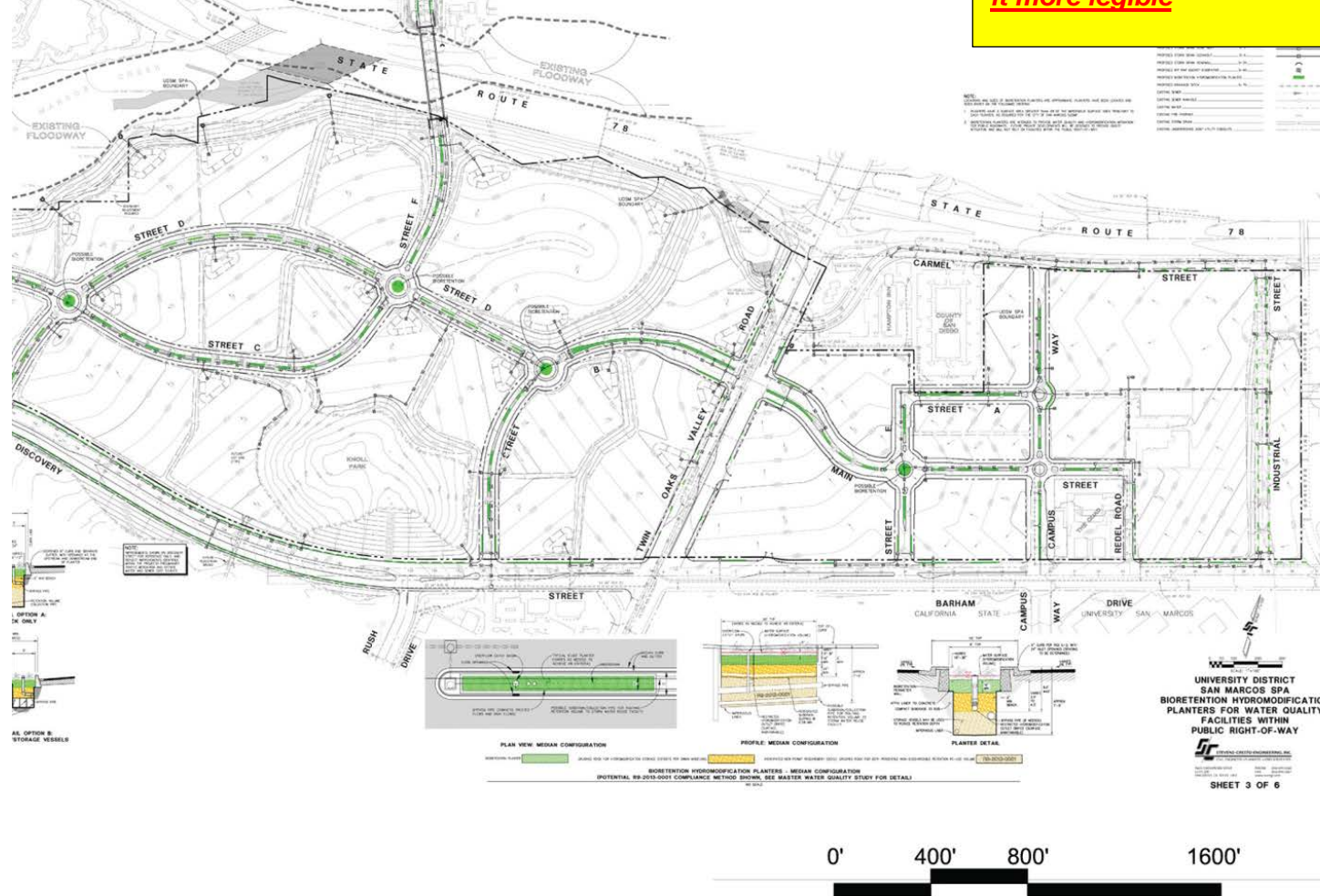


Figure VI.E: LID in Public R.O.W. (East)

Figure to be revised to make it more legible

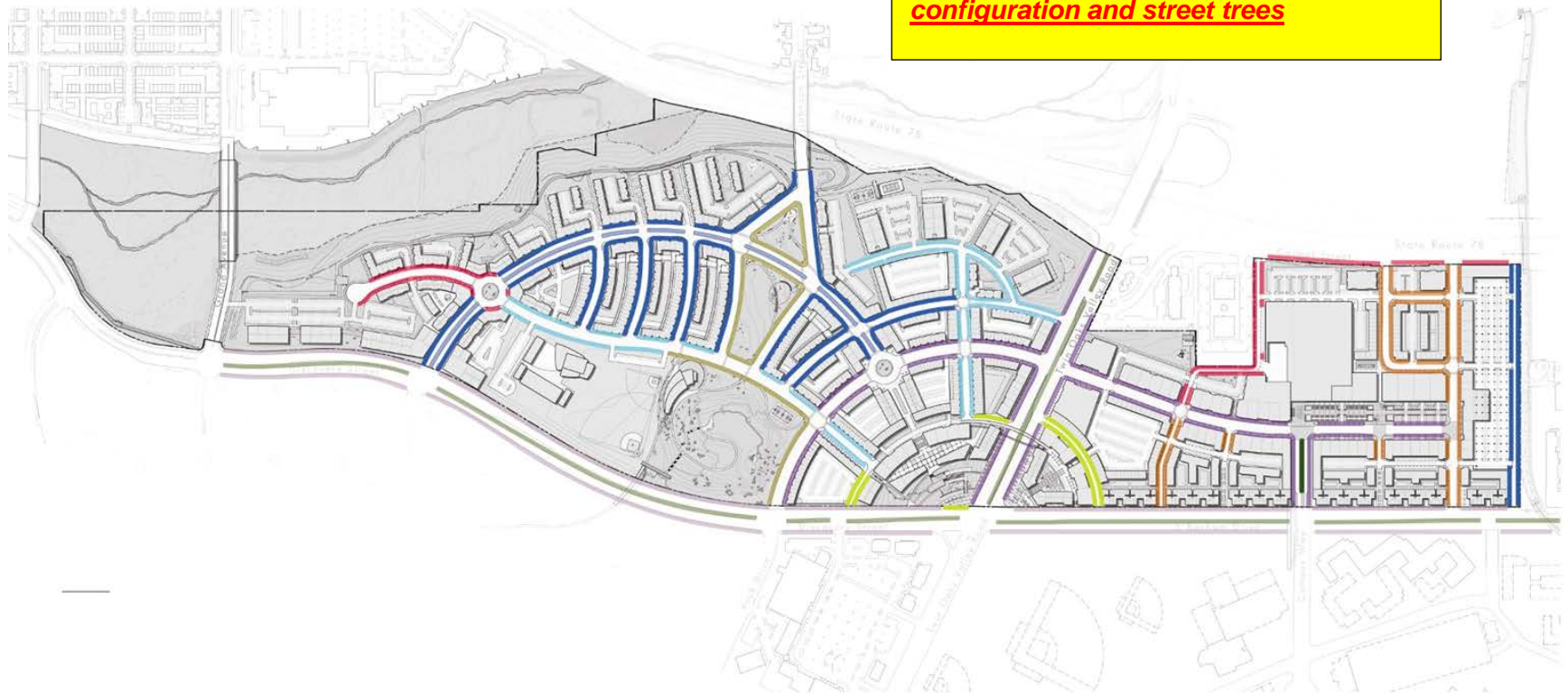


Tree Grate Flow Through Planter



FIGURE VI.F: Street Tree Diagram

Figure to be update with revised configuration and street trees



LEGEND

Conceptual Street Tree Palette

	Arbutus 'Marina' (Madrone hybrid)		Quercus agrifolia (Coast Live Oak)		Native Bioswale Tree Mix:
	Lagerstroemia 'Muskogee' (Lavender Crape Myrtle)		Rhus lancea (African Sumac)		Alnus rhombifolia (White Alder)
	Phoenix dactylifera (Medjool Date Palm)		Schinus molle (California Pepper)		Platanus racemosa (California Sycamore)
	Pinus pinea (Italian Stone Pine)		Syagrus romanzoffianum (Queen Palm)		Populus fremontii (Western Cottonwood)
	Platanus racemosa (California Sycamore)		Tipuana tipu (NCN)		Salix goodingii (San Joaquin Willow)



Sidewalk and Crosswalk Pavement

Sidewalk and crosswalk pavement shall be designed in compliance with the following standards:

- ❑ All sidewalk and crosswalk pavement materials and patterns shall comply with Title 24 and the Americans with Disabilities Act.
- ❑ Paving materials shall be simple, functional, and long-lasting.
- ❑ Except where designated as enhanced paving, sidewalks in the public right-of-way shall be made of a single standard concrete mix so as to provide a uniform appearance throughout.
- ❑ Enhanced or accent paving shall be used to accentuate crosswalks, street corners, and significant pedestrian nodes. Materials may include concrete or stone unit pavers, porous, and/or integrally colored concrete with a contrasting finish, such as exposed aggregate.
- ❑ Crosswalk paving shall be enhanced to distinguish it from the surrounding road. It shall be of a material that will not present difficulties to people with decreased mobility and shall meet the safety criteria established by the City of San Marcos and any other regulating authority.
- ❑ A simple, uniform score joint pattern that is coordinated with the location of tree grates, light poles, building entries and other design elements shall be established for the entirety of the District to provide a cohesive appearance.
- ❑ Sidewalk and crosswalk pavement shall not be stamped to resemble another material, such as stone or brick.
- ❑ Paving shall not have beveled edges or other features that can cause excessive bouncing for carts, strollers, and wheelchairs.
- ❑ Due to maintenance and durability issues, surface colored concrete shall not be used.

Streetlights

Proposed street, sidewalk, path and trail lighting shall provide safe levels per City of San Marcos standards, and shall be configured so as not to create a new source of



substantial sky-glow, light trespass or glare that would adversely affect nighttime views in San Marcos Creek and its environs. In addition to City ordinances, lighting and luminaries shall comply with all State of California and County light pollution and energy consumption regulations and standards, including but not limited to CEQA, Title 24, and San Diego County Light Pollution Code (LPC, also known as the Dark Sky Ordinance).

Safe levels of lighting shall be provided using energy efficient lamp technologies such as metal halide, induction lamps, high pressure sodium, LED, or other light sources as approved by the City of San Marcos, along with luminaries designed with shades and cut-offs to eliminate off-site lighting and nighttime sky pollution. Lighting should be directed from edges of view corridors onto paving surfaces, rather than placing fixtures or visible sources of light in the view.

Further, energy-saving control systems should be used, including photovoltaic-sensors to turn lights off when adequate daylight is available.

Street-lights shall comply with the following standards:

- ❑ A single style of street light fixture or a family of fixture styles shall be chosen for all streets within the *University District*. The selection of the street lights shall be made in consultation with the Planning Director.
- ❑ Street light fixtures shall be made of metal with a powder coat finish. The color shall be determined in consultation with the Planning Director.
- ❑ Street lighting shall be scaled to the pedestrian, shall be provided on both sides of all streets and shall be no taller 14 feet (taller cobra head lights are allowed at intersections to meet the Public Works lighting standards).
- ❑ Street and median lighting along Barham Drive, Discovery Street, and Twin Oaks Valley Road shall complement the existing lighting fixtures found within the public right-of-way.
- ❑ Street light fixtures shall be located within three (3) feet of the face of the curb.
- ❑ Spacing of street light fixtures shall comply with City of San Marcos standards.



- ❑ Lighting fixtures for street lights should be approved low-light sources.
- ❑ Upward pointing or upward reflected light from outdoor lighting is not permitted.
- ❑ Lighting levels shall be adequate for safety, especially in areas of pedestrian crosswalks and parking, while minimizing light spillage and point of glare. Where necessary, luminaries shall have shades to shield residential areas from street lights.
- ❑ Where possible, pedestrian-scaled light poles should be combined with building and site features such as walls/fences, shade structures, traffic signal and/or other vehicular-oriented signage poles to minimize sidewalk clutter.
- ❑ Solar-powered and energy-efficient streetlights should be utilized where feasible, and incorporate time-clocks and energy management systems that automatically adjust for daily and seasonal sunrise/sunset times or simply limit lighting to specific hours of the day.

Street Furniture

Street furniture includes benches, trash/recycle receptacles, tree grates, tree guards, bollards, bike racks, drinking fountains, and bus shelters. Street furnishings are to be provided throughout the district to enhance the functionality of the streetscape and promote the use of public spaces for informal and neighborly gathering. Streets shall be designed to provide for street furniture, in accordance with the following standards:

- ❑ A single family of street furnishing styles shall be chosen for all public streets in the District. The selection of the street furniture shall be made in consultation with the Planning Director. Street furniture should be of a style that complements rather than competes with the architecture of the adjacent development.
- ❑ Street furnishings should be clustered in both areas of sun and shade to present and inviting streetscape image and encourage a wide range of seasonal use.
- ❑ Street furnishings fixtures shall be made of materials that have long life spans and are able to withstand constant use and exposure to the elements. Higher grade materials are recommended to reduce long-term maintenance and replacement costs.



- ❑ Street furniture shall be placed to maintain a clear pedestrian path.
- ❑ Trash and recycle receptacles shall be provided near high traffic areas such as parks, plazas, transit stops and retail developments. Trash and recycle receptacles shall be provided on all streets at intervals no greater than 800 feet.
- ❑ Benches shall be placed within 6 feet of the face of curb.
- ❑ Seating shall be designed in a way that discourages inappropriate use by skateboarders and stunt bikers.
- ❑ Bicycle racks should be provided near community nodes such as schools, libraries, retail developments and transit stops. Bicycle racks shall be located within public view.

Underground Parking Facilities

Underground parking structures are allowed beneath streets and public spaces provided that their location and design allows for the required street trees, landscaping features, and storm water/LID facilities. Access to utility lines shall also be considered during the design process.

Interim Parking Facilities

Surface parking lots may be allowed on an interim basis during build-out of a project, provided a permanent parking solution conforming to the requirements of this Specific Plan, and a timeframe for implementing such permanent parking solution, are identified in the project development plan application. All interim parking lots shall be landscaped and/or otherwise screened from streets and other public view sheds.

Above Ground Parking Structure Connections

Bridges that connect parking structures on adjacent blocks are allowed with the approval of the Planning Director. The bridges shall be no wider than 60' and shall be no lower than the third floor of the building.



~~B~~Above ground parking structure connection bridges shall only be allowed to span over the following street types:

- ~~□ Street Type F~~
- Street Type G
- ~~□ Street Type H~~
- ~~□ Street Type I~~

Parking Structure Special Urban Design Standards

To better accommodate more urban development, the following standards apply to parking structures within the *University District*:

- Standard stall dimensions for retail and restaurant parking shall be a minimum of 8.5 feet wide by 18 feet long.
- Standard stall dimensions for residential parking shall be a minimum of 8 feet wide by 18 feet long.
- Compact stall dimensions for commercial parking shall be a minimum of 8 feet wide by 18 feet long. A maximum of 20 percent of the parking spaces may utilize these compact parking stall dimensions; the remainder shall comply with the standard stall dimensions of 8.5 feet wide by 18 feet long.
- Drive aisles shall be a minimum of 24 feet wide for 90 degree (perpendicular) parking and a minimum of 26 feet wide along end aisles to allow for an efficient turning radius in both directions. Perpendicular parking is the most efficient and is preferred over angled or parallel parking.
- Ramps shall have a maximum slope of 6 percent.

VI.5 Public Park and Gathering Space Standards

This section addresses the design of all public spaces (including spaces on private property that are open to the general public) within the *University District*. Additional landscaping standards for private spaces are provided in Section VI.9 of this Chapter.

Intent

The *University District* is envisioned to be developed with a variety of public spaces. These spaces range in approximate size from 0.75-acre mini-parks to a nearly 9-acre neighborhood park. These spaces are intended to:

- ❑ Provide easily accessible recreation areas and to provide visual breaks in the built environment.
- ❑ Provide links to pedestrian and bike trails and function as nodes and rest areas along the greater San Marcos trail system.
- ❑ Create gathering spaces for residents and visitors that are inviting, enhance the overall character of the District, and serve a variety of users with passive and active recreational options.
- ❑ Promote walking and bicycling within the District by including a series of pathways that connect to the various neighborhoods, areas, trails, parks and plazas that make up the District.
- ❑ Preserve prominent natural features that are valuable assets and tie the District to its unique context.
- ❑ Connect the District's urban open spaces to the larger community by providing clear and convenient connections to Cal State San Marcos and other neighboring developments and public spaces.
- ❑ Maximize opportunities for low impact development (LID) measures to manage storm water run-off.
- ❑ Comply with City of San Marcos design guidelines for Park and Recreation amenities

General Public Space Standards

Standards for the public spaces in the *University District* are provided on the following pages. The locations of these public spaces, provided on Figure VI.B: Building and

Public Space Regulating Plan, are conceptual only and represent approximate sizes, configurations, as well as range of recreational uses and amenities.

Please reference Chapter IV – Open Space | Conservation for additional descriptions of the various public parks and open spaces conceptualized within the *University District* project.

East Urban Plaza~~s~~

The East Urban Plaza~~s~~ shall ~~occupy approximately 2.0 acres, with a minimum dimension of one hundred (100) feet.~~

T~~include~~ the following elements ~~are required within the East Urban Plaza~~s~~:~~

- ❑ Space for outdoor cafés and seating areas
- ❑ Site furnishings including fixed and moveable seating, trash receptacles, bike racks, and pedestrian scaled lighting
- ❑ A pedestrian paseo connection to the blocks and developments north of the plaza
- ❑ Canopy s~~shade~~ and accent trees at a maximum of forty (40) feet on center, excluding in areas of driveway curb cuts and utility vaults
- ❑ Storm water management features such as permeable paving, bio-retention and self-retaining areas, sized at a minimum to accommodate run-off from plaza hardscape areas
- ❑ Distinctive, enhanced plaza paving, consistent with the surrounding commercial and residential development
- ❑ Community event space for performances, festivals, and multi-use gathering space~~40-stall fair or farmer's market~~

The following elements may also be provided within the East Urban Plaza~~s~~:

- ❑ Public art elements
- ❑ Interactive water feature
- ❑ Shade structures, kiosks or arcades
- ❑ Food carts and mobile vendors
- ❑ Informational kiosks
- ❑ Area for farmer's markets



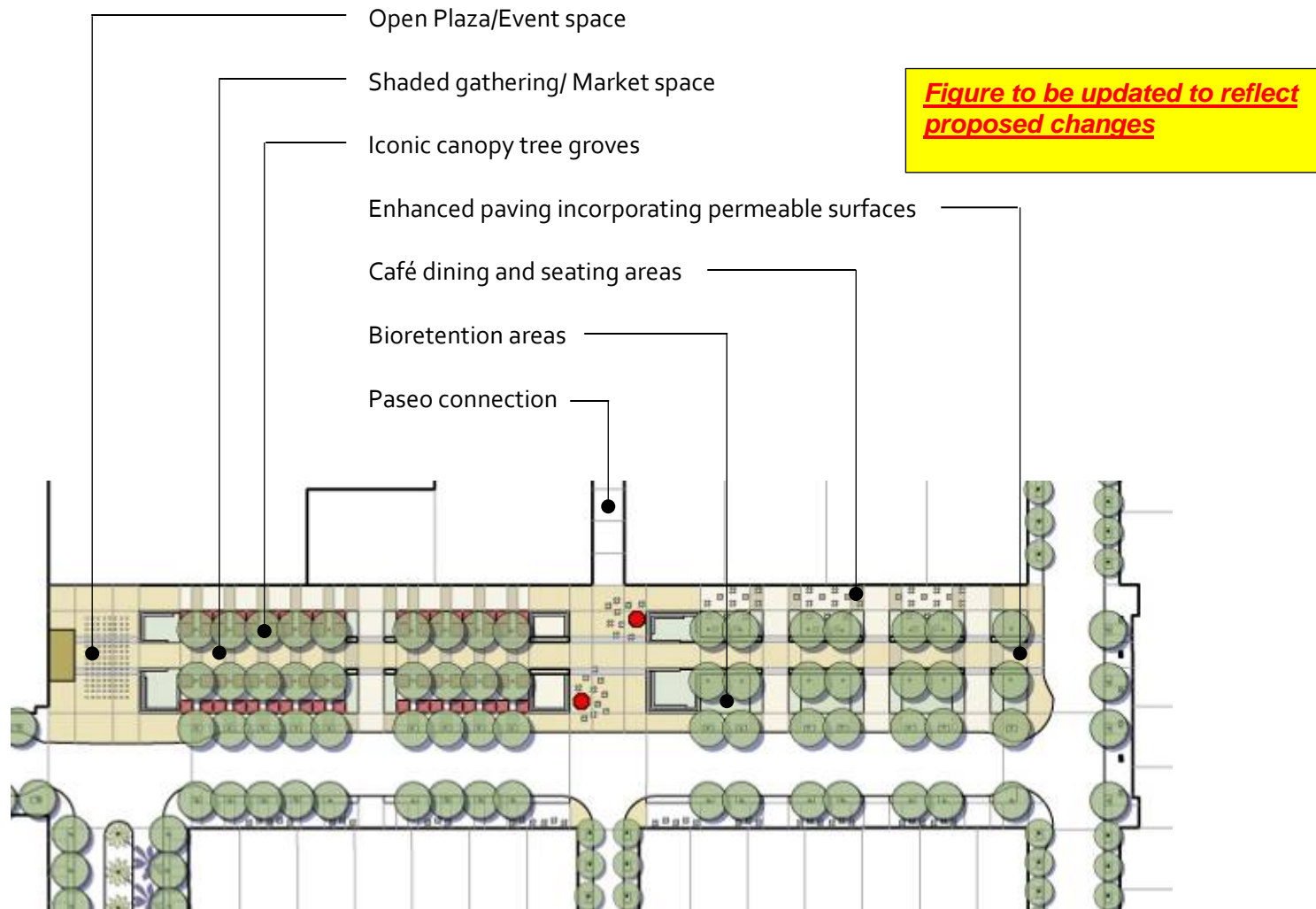


FIGURE VI.G: East Urban Plaza Enlarged Plan

0' 50' 100' 200'

|

East and West Paseos

Open Plaza/Event space

Shaded gathering/Market space

Iconic canopy tree groves

Paseos are linear, pedestrian-only circulation zones that provide opportunities for small scale urban plazas and gathering spaces. Paseos shall be a minimum of forty (40) feet wide. Paseo hardscape and plaza areas shall be configured to allow access for fire trucks where applicable.

Enhanced paving incorporating permeable surfaces

The following elements are required within each paseo:

Café dining and seating areas

- ❑ Storm water management features such as permeable paving and bio-retention and self-retaining areas, sized at a minimum to accommodate run-off from paseo hardscape areas
- ❑ Canopy accent trees and enhanced feature planting
- ❑ Enhanced pavement that distinguishes the paseo from adjacent public sidewalks
- ❑ Site furnishings including seating, trash receptacles, bike racks, and pedestrian scaled lighting

Bioretention areas

Paseo connection

The following elements may also be provided within paseos:

- ❑ Public art elements
- ❑ Interactive or reclaimed water features
- ❑ Moveable or café seating areas
- ❑ Informational kiosks
- ❑ Coffee cart and mobile vendors
- ❑ Small scale activities such as game tables, climbing structures, bocce courts, etc.



FIGURE VI.G: East Urban Plaza Enlarged Plan

0' 50' 100' 200'

Figure to be updated to reflect proposed changes

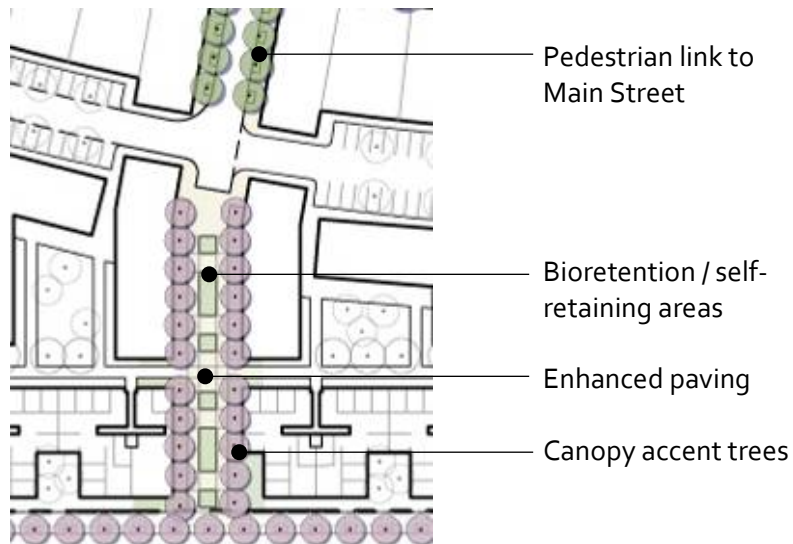


FIGURE VI.H: Typical East Paseo Conceptual Plan

0' 50' 100' 200'

Figure to be updated to reflect proposed changes

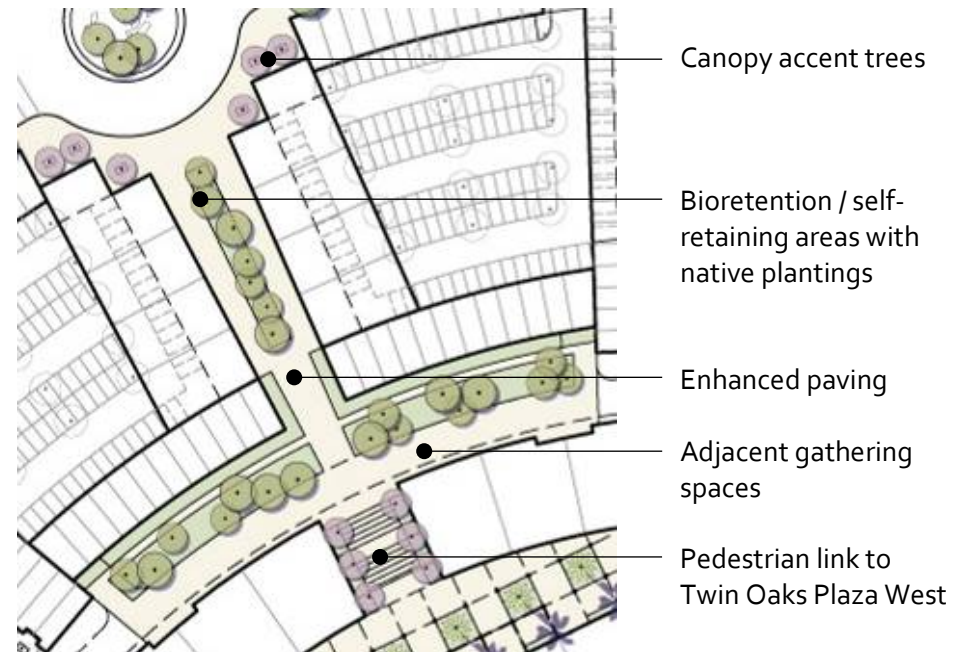


FIGURE VI.G: Typical West Paseo Conceptual Plan

0' 50' 100' 200'

East Green

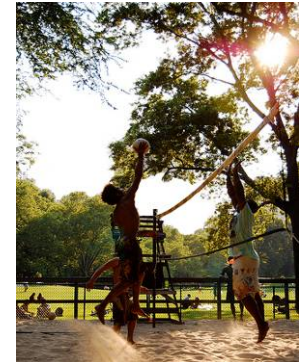
The East Green ~~shall occupy approximately 1.5 acres. It~~ is a primarily softscape recreational space and due to its location as a relative low point within the district, could potentially function as a stormwater infiltration site.

The following elements are required within the East Green:

- ❑ Storm water management features including vegetated self-retaining or bio-retention area, designed to accommodate recreational activity during non-storm events
- ❑ Pervious paving at all paths , plazas and play areas
- ❑ Shade structure and pervious surface plaza
- ❑ ~~Minimum .75 acre o~~Open field play area
- ❑ Site furnishings including seating, trash receptacles, bike racks, and pedestrian scaled lighting
- ❑ Mini-plazas at pedestrian paseo linkages

The following elements may also be provided within the East Green:

- ❑ Small-scale, soft surface recreational features such as a climbing wall, interactive water feature, bocce or lawn-bowling
- ❑ Fenced dog run
- ❑ Small playground or tot-lot
- ❑ Barbecue grills, picnic tables and ash receptacles



|

Figure to be updated to reflect proposed changes



FIGURE VI.J: East Green Conceptual Plan

0' 50' 100' 200'

Twin Oaks Plaza (East)

Twin Oaks Plaza (East), ~~it shall occupy approximately 0.251.2 acres.~~ With Twin Oaks Plaza West, it forms the symbolic south gateway to the District.

The following elements are required within Twin Oaks Plaza (East):

- ❑ Major accent trees, coordinated with the design of the plaza to the west, that form a focal point for the gateway and compliments the existing plantings on the south side of the intersection
- ❑ Storm water management features such as self-retaining areas, pervious paving, rain gardens or flow through planters
- ❑ Enhanced paving at the pedestrian crosswalk and mini plazas that is coordinated with the design of the plaza to the west and compliments the existing enhanced paving in the intersection
- ❑ Site furnishings including seating, trash receptacles, bike racks, and pedestrian scaled lighting
- ❑ Wayfinding graphics and signage

The following elements may also be provided within Twin Oaks Plaza (East):

- ❑ Public art element
- ❑ Water feature
- ❑ Café seating area
- ❑ Enhanced/ accent lighting effects
- ❑ Information or small retail kiosks



Figure to be updated to reflect proposed changes

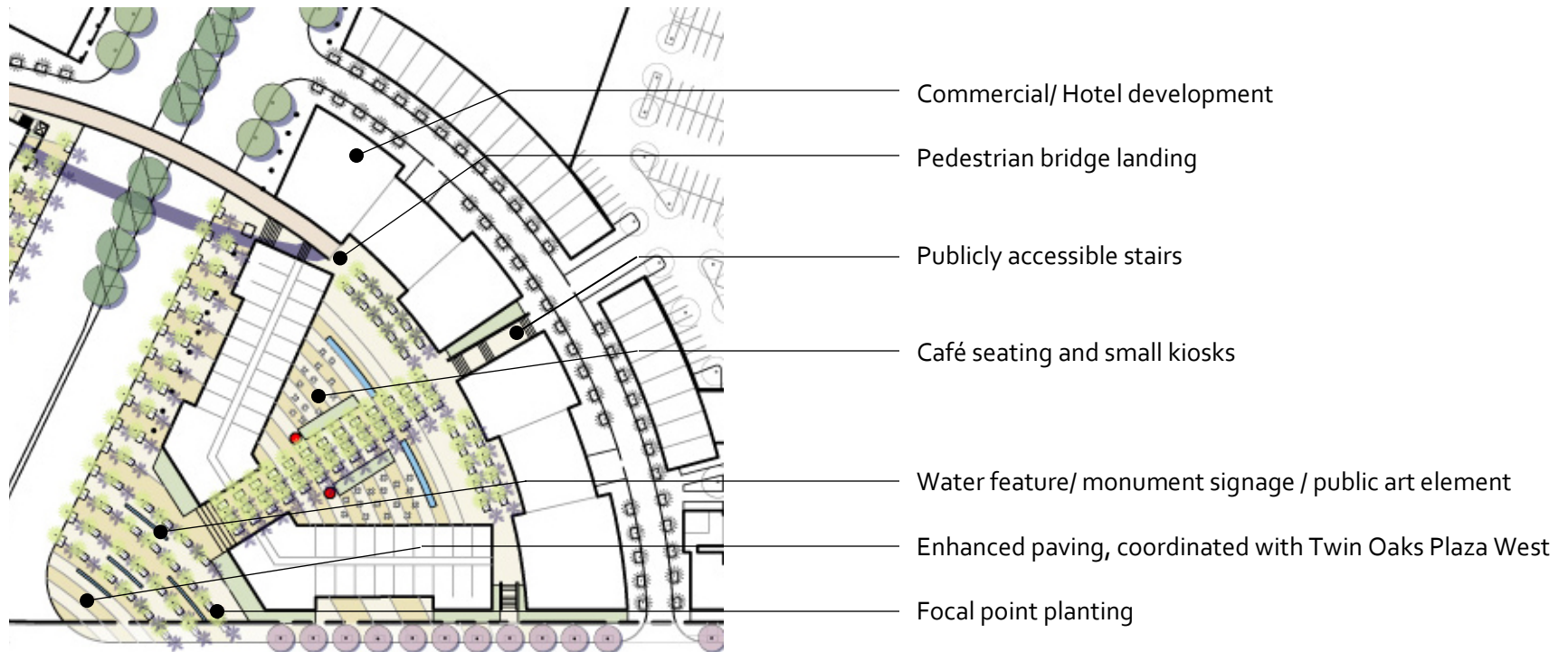


FIGURE VI.K: Twin Oaks Plaza (East) Conceptual Plan

0' 50' 100' 200'

Twin Oaks ~~West~~ Plazas and Paseos (~~West~~)

These urban plazas and paseos are located north and west of the Twin Oaks Valley Road and Discovery Street/Barham Drive intersection. The primary purpose of these plazas and paseos is to provide strong pedestrian connectivity across Twin Oaks Valley Road and to mixed-use development on either side of the street. In concert with Twin Oaks Valley East Plaza, these plazas and paseos form the southern gateway to the District and serve to anchor the bridge across Twin Oaks Valley Road. Landscaping, monument signage, artwork, weater features, or architecturally significant building elements would distinguish these plazas and paseos, including at the northwest corner of the Twin Oaks Valley Road and Discovery Street/Barham Road intersection. Twin Oaks Plaza (West) shall occupy approximately 2.70 acres. Combined with Twin Oaks Plaza (East), this mixed-use area forms the symbolic south gateway to the University District.

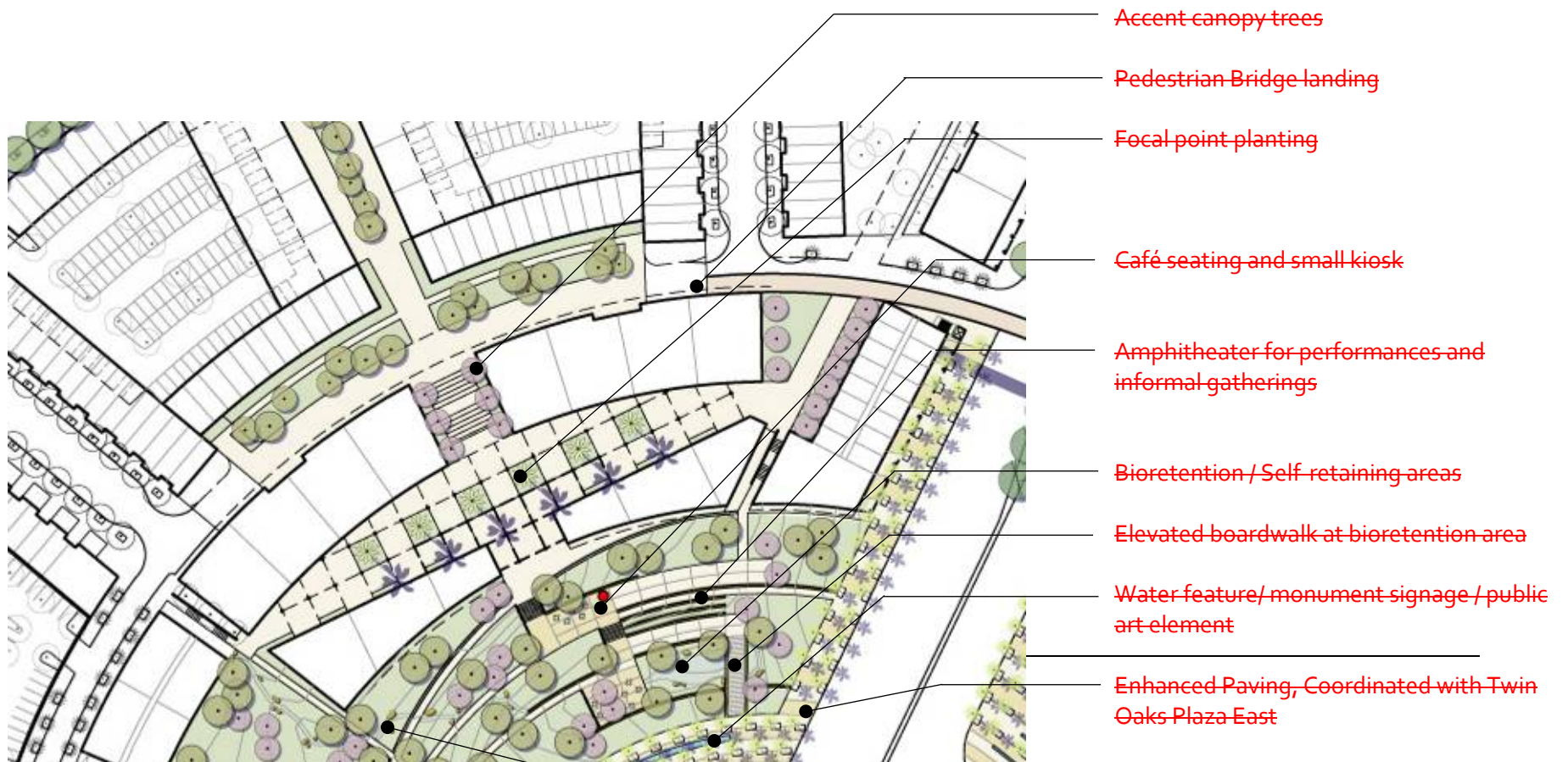
The following elements ~~are required~~ may be provided within Twin Oaks West Plazas and Paseos (~~West~~):

- ❑ Major accent trees, coordinated with the design of the plaza to the east, that form a focal point for the gateway and are consistent with the existing plantings on the south side of the intersection
- ❑ Storm water management features such as self-retaining areas, pervious paving, and flow through planters
- ❑ Enhanced plaza paving that compliments the existing enhanced paving in the intersection
- ❑ Enhanced paving at ~~the~~ pedestrian crosswalks
- ❑ Site furnishings including seating, trash receptacles, bike racks, and pedestrian scaled lighting
- ❑ Wayfinding graphics and signage

~~The following elements may also be provided within Twin Oaks Plaza (West):~~



- ❑ Community event space, farmer's markets or festivals
- ❑ Amphitheater for performances and informal gathering
- ❑ Public art element
- ❑ Water feature
- ❑ Café seating area



Knoll Park and Neighborhood Greens

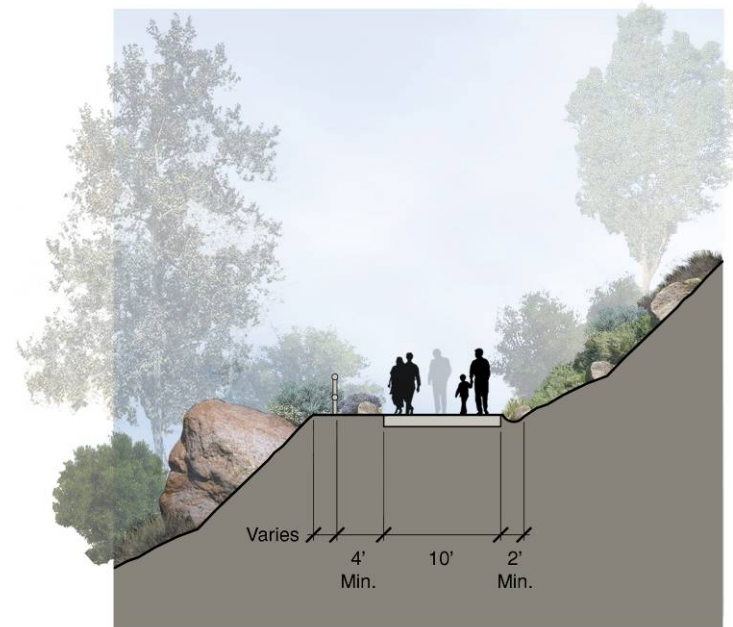
The Knoll Park and Neighborhood Greens shall occupy approximately ~~159.0~~ acres. ~~Lare~~ Lare is located centrally within the District, Knoll Park and Neighborhood Green incorporates a variety of trails and active uses into the existing and distinctive granitic topographic feature.

The following elements are required within the Knoll Park and Neighborhood Greens:

- ❑ Stabilized decomposed granite or other soft surface multi-use trail with maximum 1:20 slope
- ❑ Picnic seating and BBQ areas
- ❑ Rest areas with seating along trail at a maximum distance of 1,200 feet
- ❑ Observation point
- ❑ Minimum .75 acre open field or sport court play area at base of knoll
- ❑ Minimum .75 acre open play field area at top of knoll
- ❑ Community Center
- ❑ Full-size basketball court
- ❑ Two tennis courts which can also accommodate pickle ball
- ❑ Two tot lots (one for ages 2 to 5 years and one for 5 to 12 years)
- ❑ Restroom
- ❑ Skateboard park
- ❑ Multi-purpose field (minimum of 180 feet by 320 feet)
- ❑ Downhill mountain bike path on the knoll

The following elements may be included within the Knoll Park and Neighborhood Greens:

- ❑ Recreational elements that make use of the rugged terrain, including Disc golf course, climbing wall, water fall, adventure playground, or bouldering trail



- ❑ Interpretive exhibits and signage about regional ecology and the environment
- ❑ Botanical Garden featuring specimen examples of native plant communities
- ❑ User Orientation Kiosk



Figure to be updated to reflect proposed changes

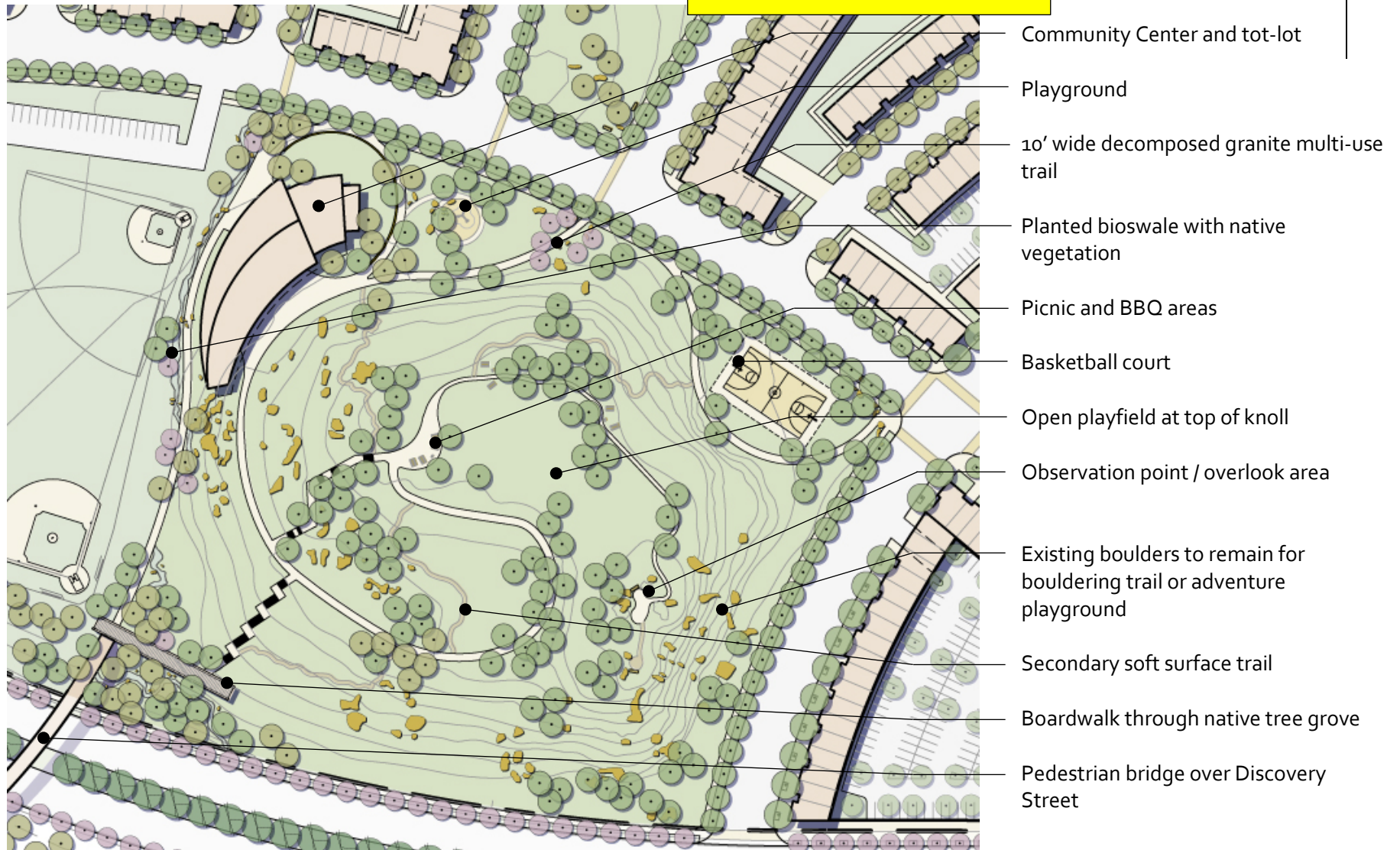


FIGURE VI.ML: Knoll Park Conceptual Plan

0' 50' 100' 200'

South-Neighborhood Green

The South-Neighborhood Green shall occupy approximately 1.6 acres. In concert with the North-Neighborhood Green and Knoll Parks, it forms the community's central park and recreational district. The character of these three parks should be consistent, well-coordinated and respond to the native plant and material palettes of San Marcos Creek and the surrounding hills.

The following elements are required within the South-Neighborhood Green:

Minimum .75-acre open field play area

Stabilized decomposed granite or other soft surface path linking Knoll park to North-Community Green

Enhanced paving at entry nodes

Site furnishings including seating, trash receptacles, bike racks, and pedestrian-scaled lighting

Iconic grove of native trees such as *Quercus agrifolia* or *Platanus racemosa*

Shaded picnic and seating areas

The following elements may also be provided within the South-Neighborhood Green:

Basketball or other sport court

Playground or tot lot

Interactive water play area

Barbecue facilities



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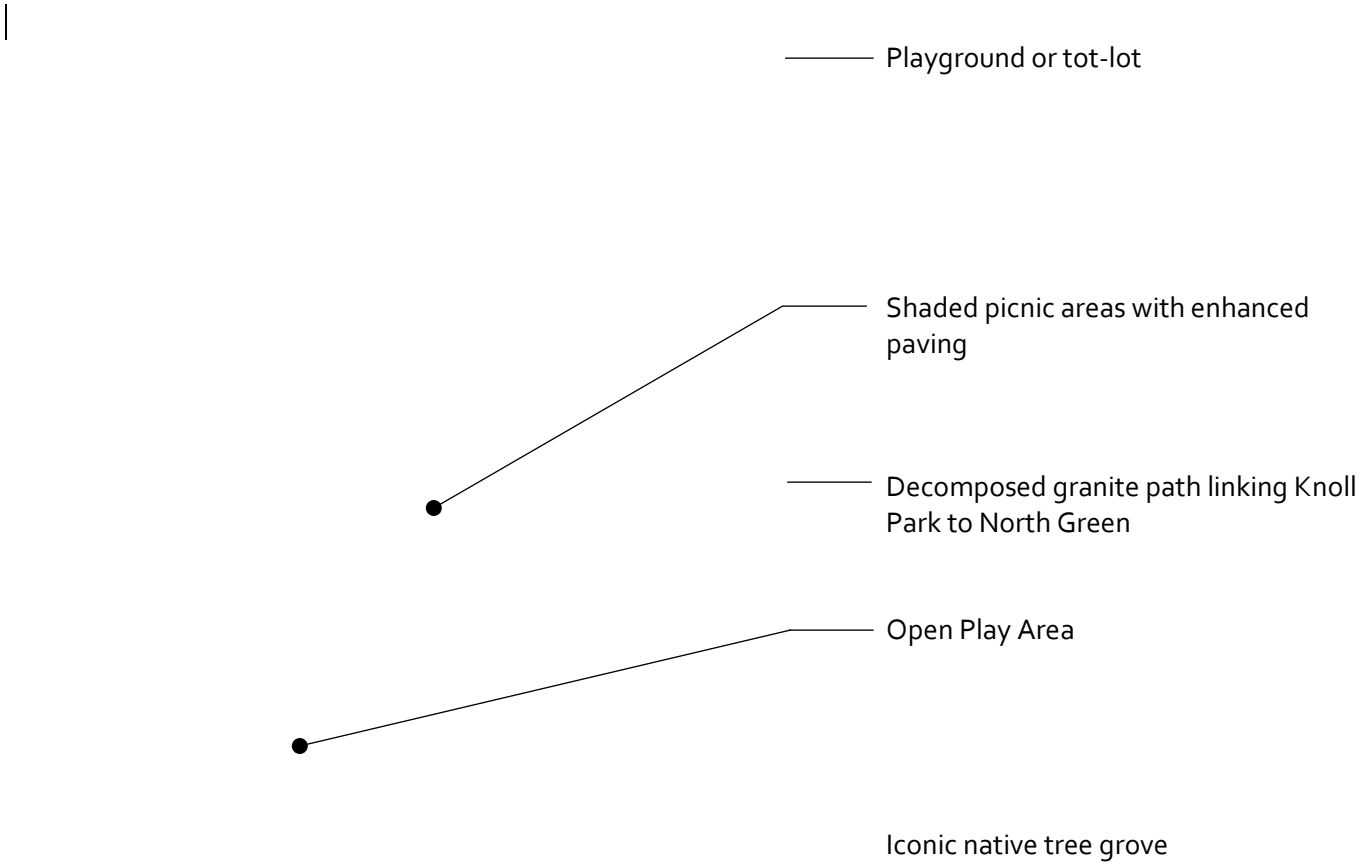


FIGURE VI.N: South Neighborhood Green Conceptual Plan



North Neighborhood Green

The North Neighborhood Green shall occupy approximately 1.1 acres. In concert with the South Neighborhood Green and Knoll Parks, it forms the community's central park and recreational district. The character of these three parks should be consistent, well coordinated and respond to the native plant and material palettes of San Marcos Creek and the surrounding hills.

The following elements are required within the North Neighborhood Green:

Stabilized decomposed granite or other soft surface path

Enhanced paving at entry nodes

Site furnishings including seating, trash receptacles, bike racks, and pedestrian scaled lighting

Minimum 2,500 square foot mini plaza

Iconic grove of native trees such as *Quercus agrifolia* or *Platanus racemosa*

The following elements may also be provided within the North Neighborhood Green:

Botanical or strolling garden

Interactive water feature

Tot lot, water play area or small bouldering field

Small scale sport court such as lawn bowling, bocce or horse shoes



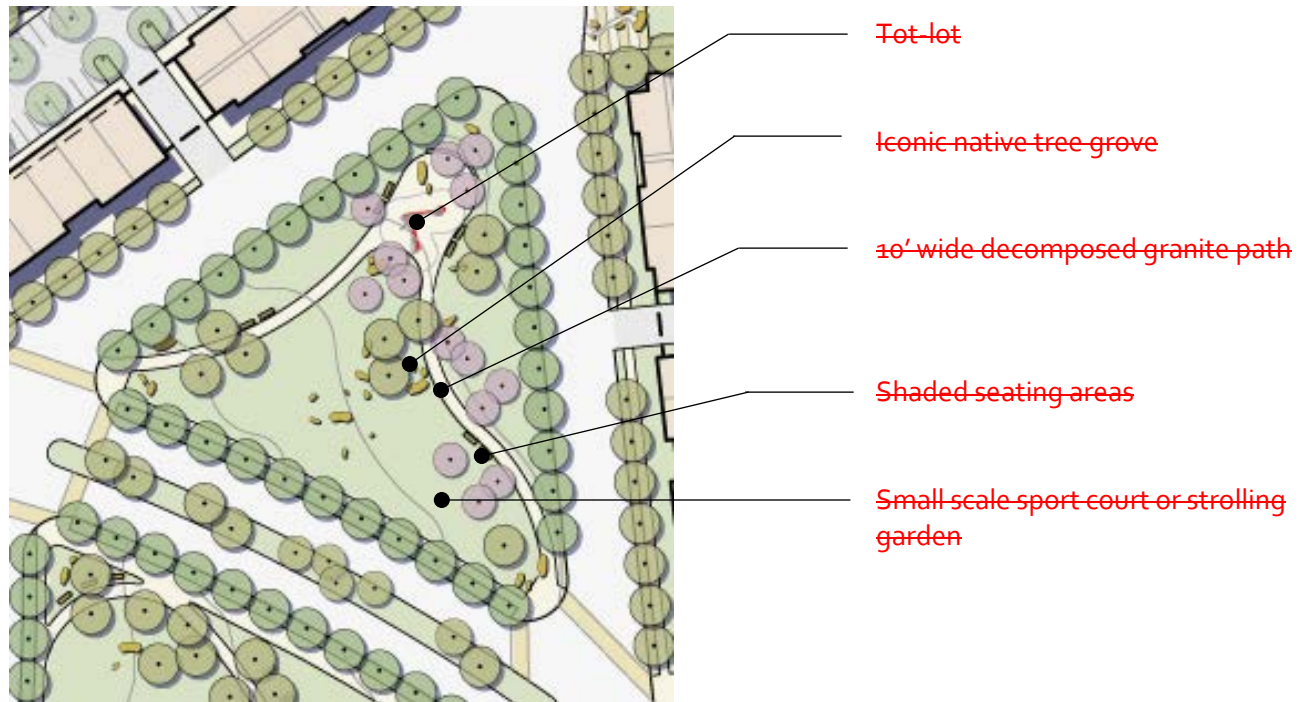


FIGURE VI.O: North Neighborhood Green Conceptual Plan

0' 50' 100' 200'

Creek Trail / West Creek Park

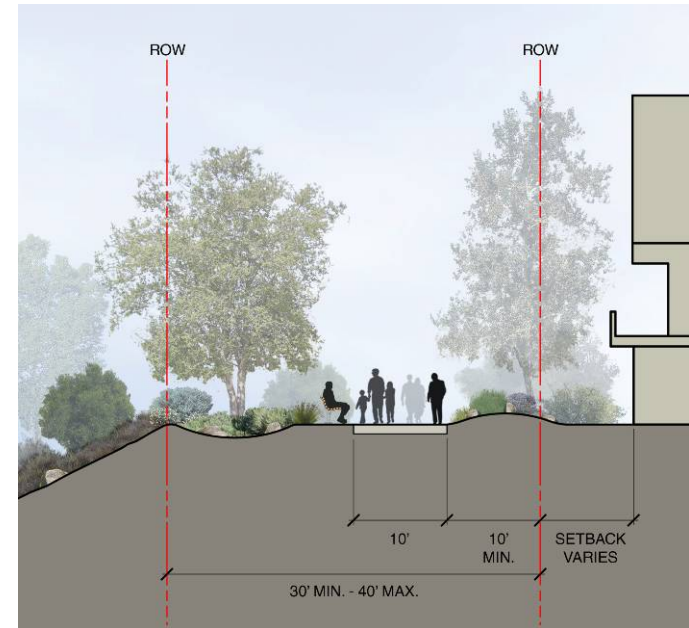
The West Creek Park and trail ~~provide a total of approximately 3.2 acres of park space. These spaces~~ serve as the transition from the San Marcos Creek riparian corridor to the development, and should blend these areas together through the use of native plantings.

The following elements are required within the Creek Trail and West Creek Park:

- ❑ 10' wide multi-use trail with stabilized decomposed granite or other pervious, soft surface meeting city of San Marcos Park and Recreation design guidelines
- ❑ Storm water management features including greenswards graded to function as self-retaining areas, bio-retention areas in vegetated buffer between public and private areas
- ❑ Open field play area
- ❑ Site furnishings, including benches spaced at a maximum of 600 feet, trash receptacles and bike racks at gateways and nodes
- ❑ Low-level pedestrian scaled lighting at high-use areas
- ❑ Picnic area/ barbecue facilities

The following elements may also be provided within the Creek Side Trail and Open Space:

- ❑ Fenced dog run
- ❑ Fitness stations or other compact recreational equipment including play equipment, adventure trails, etc.
- ❑ Overlooks, bridges and/or boardwalks to promote a more interactive experience in the riparian corridor
- ❑ Interpretive exhibits and signage about regional ecology and environment
- ❑ Community garden plots or Botanical Garden



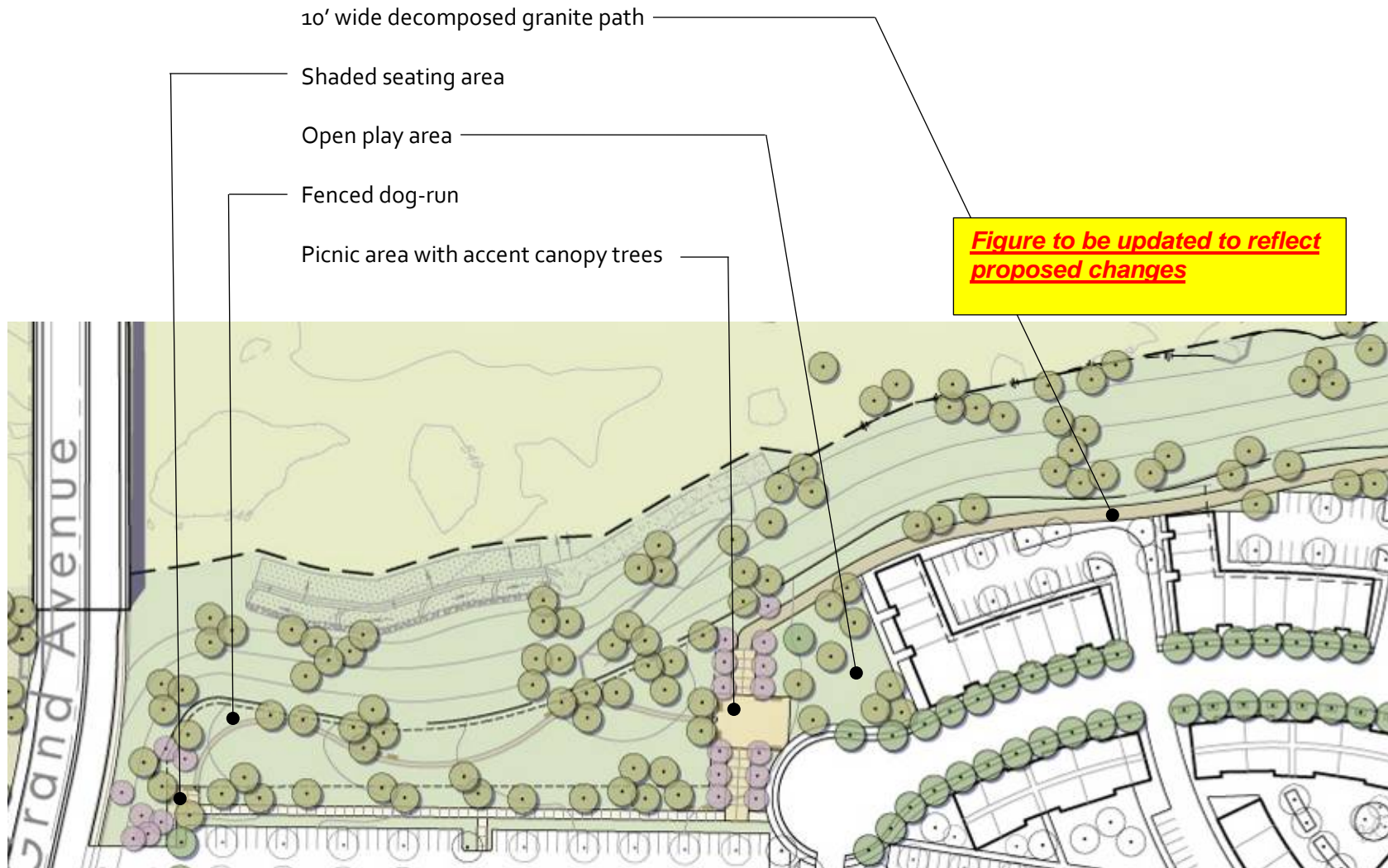


FIGURE VI.IMP: Creek Trail / West Creek Park Conceptual Plan

Creek Trail / East Creek Park

The Creek Trail and East Creek Park ~~provide a total of approximately 3.2 acres of park space and .75 miles of trail. These spaces~~ serve as the transition from the San Marcos Creek riparian corridor to the development, and should blend these areas together through the use of native plantings.

The following elements are required within the Creek Trail and East Creek Park:

- ❑ Ten (10) foot wide multi-use trail with stabilized decomposed granite or other pervious, soft surface meeting city of San Marcos Park and Recreation design guidelines
- ❑ Storm water management features including greenswards graded to function as self-retaining areas, bio-retention areas in vegetated buffer between public and private areas
- ❑ Open field play area
- ❑ Site furnishings, including benches spaced at a maximum of six hundred (600) feet, trash receptacles and bike racks at gateways and nodes
- ❑ Low-level pedestrian scaled lighting at high-use areas
- ❑ Picnic area/ barbecue facilities

The following elements may also be provided within the Creek Side Trail and Open Space:

- ❑ Fenced dog run (where one-third of the area is for small dogs and two-thirds is for large dogs)
- ❑ Fitness stations or other compact recreational equipment including play equipment, adventure trails, etc.
- ❑ Overlooks, bridges and/or boardwalks to promote a more interactive experience in the riparian corridor
- ❑ Interpretive exhibits and signage about regional ecology and environment
- ❑ Community garden plots or Botanical Garden



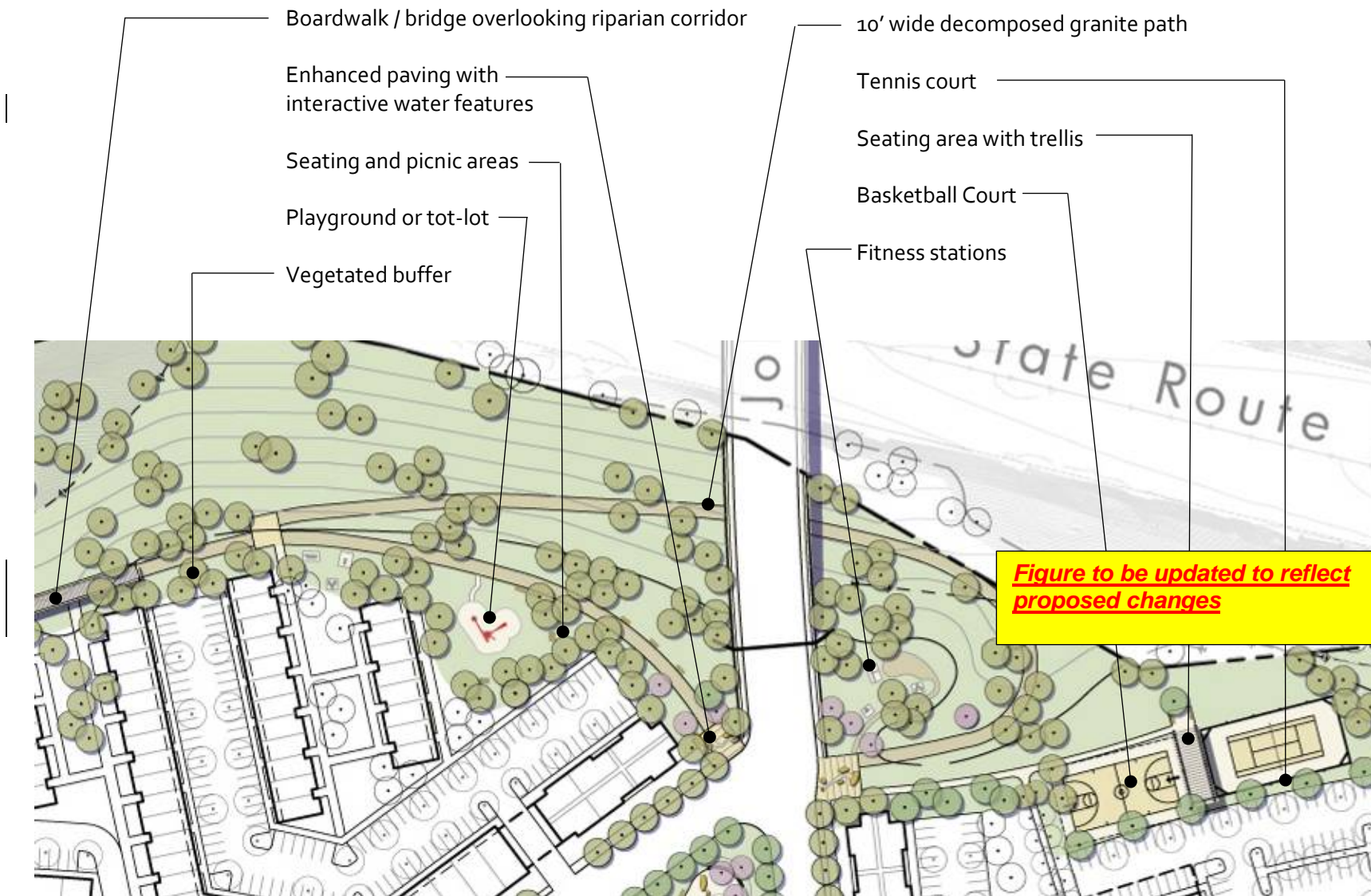


FIGURE VI.QN: Creek Trail / East Creek Park Conceptual Plan

0' 50' 100' 200'

Westland Trail / Open Space

A ~~nearly 1,200-foot-long~~ boardwalk trail is proposed for the ~~10.0-acre~~ open space area located at the far west end of the District, west of Grand Avenue. The Wetland Trail and Open Space will serve as a major connection for the district to neighboring development through trail and natural space connections.

The following elements are required within the Wetland Trail and Open Space:

- ❑ Ten (10) foot wide elevated boardwalk to promote an interactive experience within the riparian corridor with minimal disruption to the native vegetation
- ❑ Fully accessible connections to existing and proposed city wide urban trails at Discovery Street and Grand Avenue
- ❑ Interpretive exhibits and signage about regional ecology and environment
- ❑ Seating areas



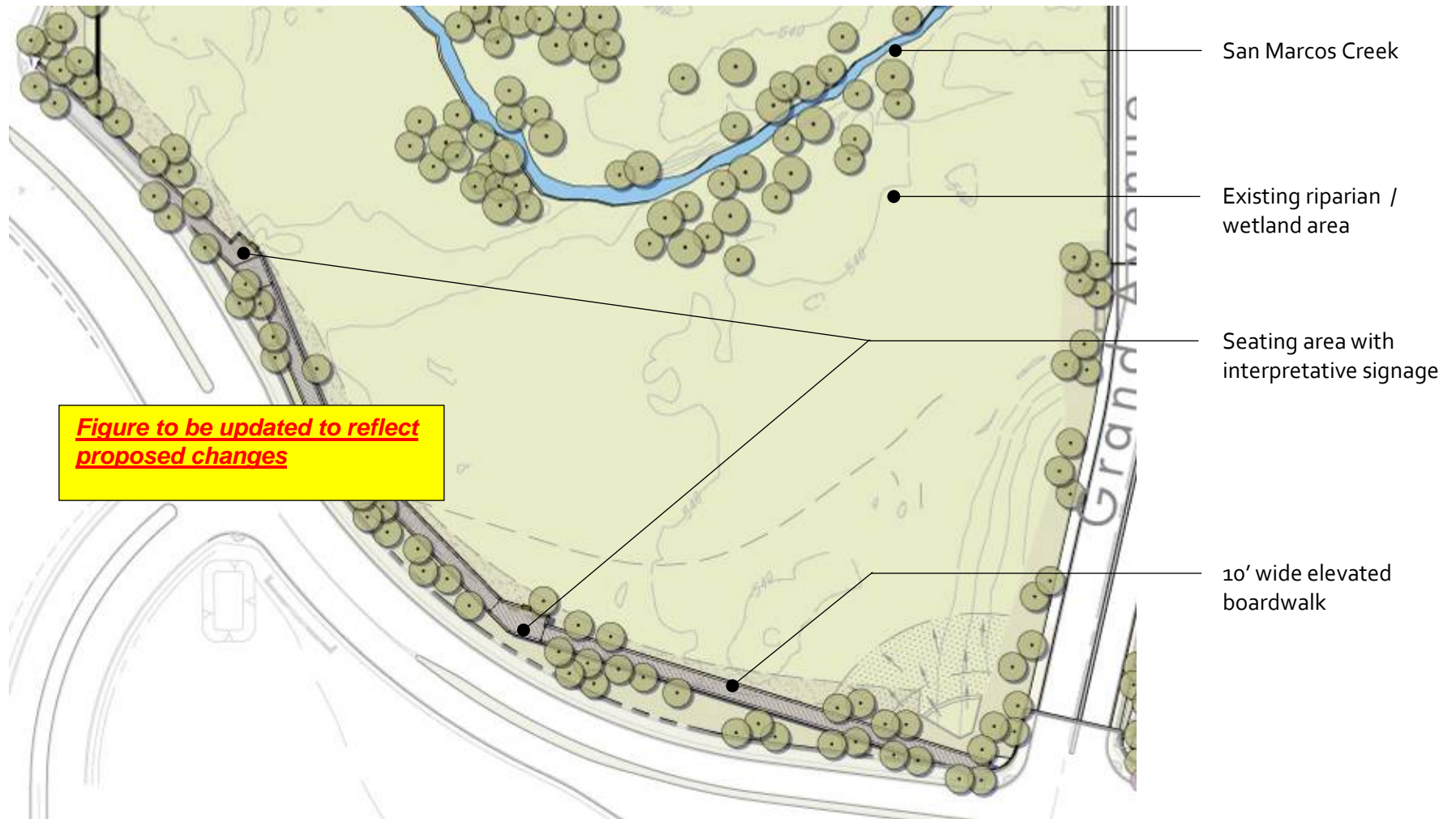


FIGURE VI. ~~JOR~~: Wetland Trail / Open Space Conceptual Plan

0' 50' 100' 200'

VI.6 Building Type Standards

Intent

The *University District* is envisioned to be developed with a variety of building types. These building types are intended to:

- ❑ Define the street edges as attractive public spaces.
- ❑ Create a positive relationship between internal building uses and activities along streets and public space.
- ❑ Provide opportunities for mix of compatible commercial, residential, and civic uses.

Building Standards

Within the *University District*, a variety of building types are envisioned and allowed. These building types include:

- ❑ Mixed-Use Building A
- ❑ Mixed-Use Building B
- ❑ Commercial Building
- ❑ Freeway Commercial Building A
- ❑ Freeway Commercial Building B
- ❑ University Flats
- ❑ Office Flats
- ❑ Townhomes/Flats
- ❑ Creek Side Townhomes/Flats
- ❑ Adaptive Reuse Buildings
- ❑ Civic Buildings

Development standards for these building types are provided on the following pages. For each building type, there are standards that address lot size and building

placement, vehicle access and parking, building height and mass, frontages, building uses, and parking requirements.

Additional design and landscaping standards that apply to all building types are provided in the following sections of this Form-Based Code:

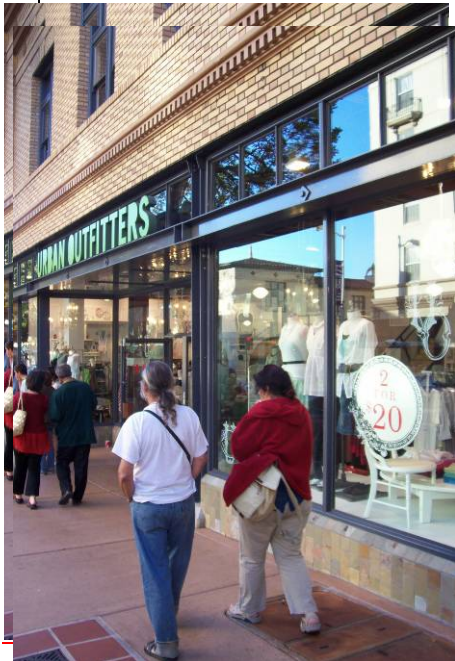
- ❑ VI.7 (Frontages and Projections)
- ❑ VI.8 (Architectural Standards and Guidelines)
- ❑ VI.9 (General Landscaping Standards)
- ❑ VI.10 (Business Sign Standards)

Development Standards for Mixed-Use Building A

Description

Mixed-Use Building A: a multi-story building that contains a mix of commercial and residential uses. Along the front façade, the ground floor generally contains storefronts for retail, dining, and entertainment uses. Upper floors generally contain residential units or office uses.

~~The development sites located to the north of and fronting the East Urban Plazas and/or along Street Type E-2 (Two Lane Collector Street) may be developed with an anchor retail store building (any commercial building with a footprint greater than 50,000 square feet). The anchor retail store building would only be allowed if it is partially concealed by mixed use buildings placed in front of the anchor retail store.~~



Mixed-Use Building A: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller properties.

A Lot Width 20' minimum

B Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

All Property Lines A setback is not required.

C Exception An 8' setback is required from external side property lines along Street Type L.

Build-to-Line (BTL)

D Build-to-Line Building facades shall be placed along the front and external side property lines.

E Exception A Facades shall be placed within 8' to 12' of the property line along Street Type L.

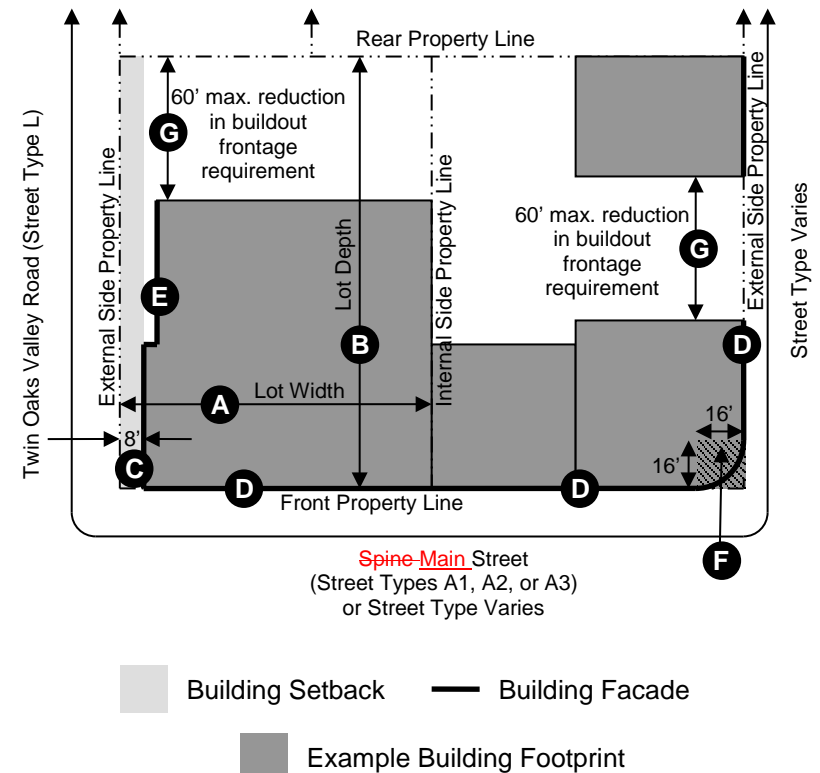
F Exception B At block corners, the front facade may deviate from the BTL within an area that is not greater than 16' by 16' square.

Exception C The BTL may be set back to allow for forecourts.

G Frontage Buildout 100% of the site's total street frontage (as measured by the total length of front and external side property lines) shall be occupied by one or more building facades.

Exception A The frontage build out requirement may be reduced along side property lines at internal side streets (excluding those fronting Twin Oaks Valley road) by 60' to allow for driveways and parking lots.

Exception B For properties at the corner of Street Type L and A3, the frontage buildout requirement may be reduced by an additional 12' along the front property line to accommodate the setback from Street Type L.



Mixed-Use Building A: Lot Size and Building Placement: Unique Standards for East Urban Plaza_s Block

Build-to-Line (BTL)

- A** Build-to-Line of Mixed-Use Buildings Mixed-use buildings are allowed along all street frontages. They are required along Street Type A-1, the west edge of the East Urban Plaza_s, and along ~~at least 70% of the~~ north edge of the East Urban Plaza_s. The facades of these buildings shall be provided along front and external side property lines (at a 0' setback) and along the west and north edges of the Plaza.

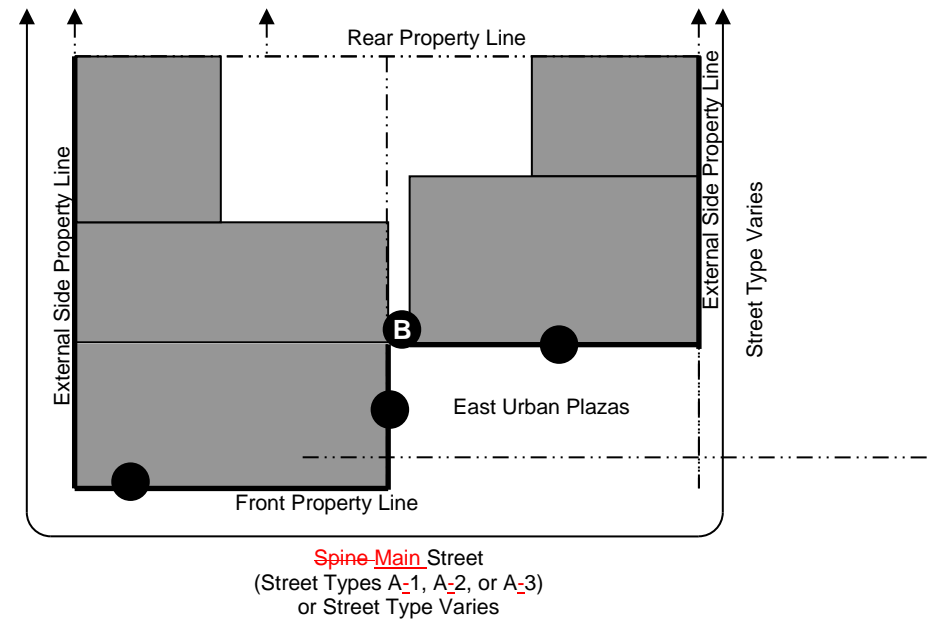
Exception A At block and plaza corners, the front facade may deviate from the BTL within an area that is not greater than 16' by 16' square.

~~Build-to-Line of Anchor Retail Store An anchor retail store is allowed north of Street Types A-1 and A-2, along Street Types E-2, F, and I and the northern edge of the East Urban Plaza_s. If provided along the Plaza, the facade shall occupy no more than 30% of the Plaza's edge. If provided along Street Type E-2, the facade shall be built along the property line (at a 0' setback). No BTL is required for Street Types F and I.~~

Frontage Buildout 100% of the total frontage along the Main Street (Street Type A-1) and the East Urban Plaza_s (as measured by the total length of front property lines along Street Type A-1 and the total length of the north and west edges of the Plaza) shall be occupied by one or more building facades.

- B** Exception The frontage build-out requirement may be reduced by 10% to create paseos that provide access from the East Urban Plaza_s to the rear properties on the block.

~~Surface Parking Surface parking is allowed along Street Types I, F and Carmel Street, and prohibited along Street Types A-1, A-2, and E-2.~~



Example Building Footprint — Building Facade

Mixed-Use Building A: Vehicle Access and Parking**Parking Options**

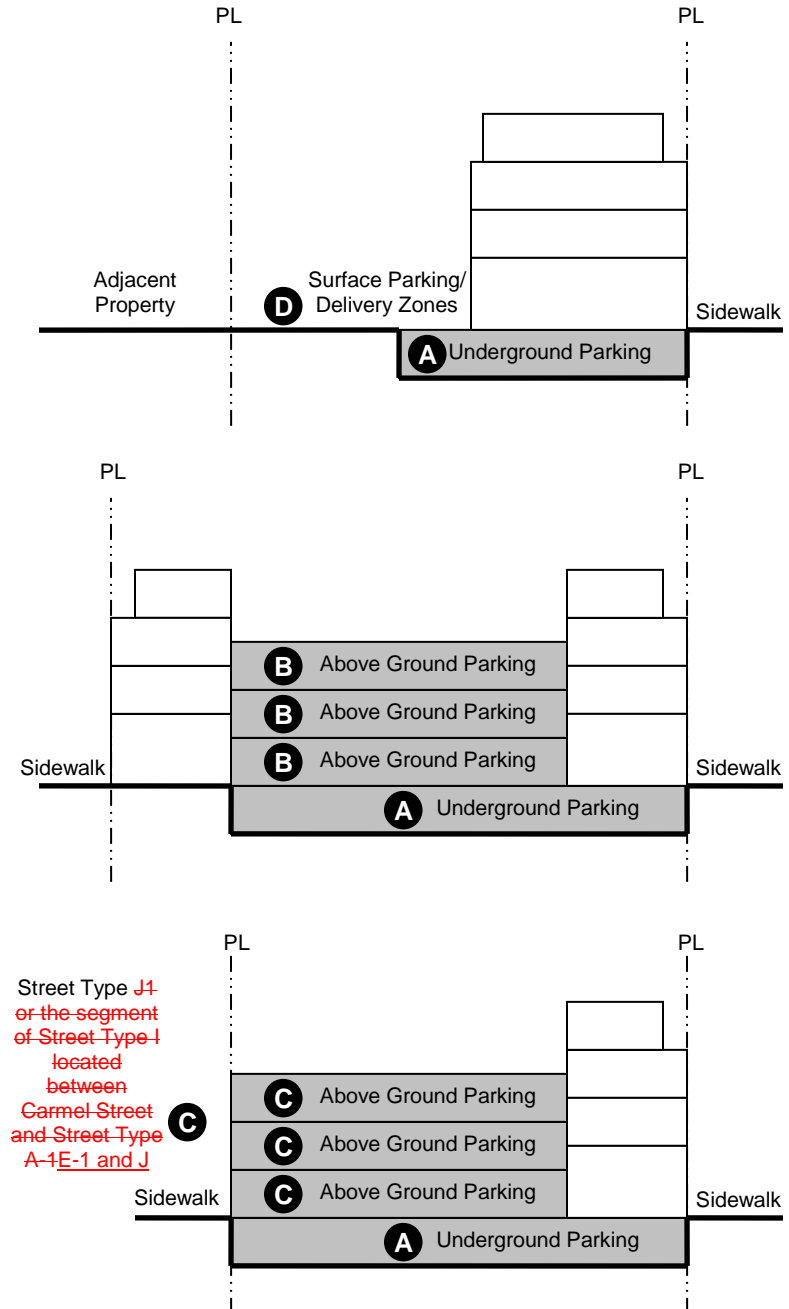
Combinations of parking options are allowed, including underground and above ground parking structures and surface parking lots.

Parking Locations

A	Underground Parking Levels	Underground parking levels are allowed on the entire site.
B	Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view by locating them to the rear of all mixed-use buildings on the block or off alleys.
C	Exception	Parking structures do not have to be screened from public view along Street Type J-1 , as well as the segment of Street Type I located between Carmel Street and Street Type A-1 <u>E-1 and J</u> .
D	Surface Parking and Delivery Zones	Surface parking lots and delivery zones are allowed if they are screened from public view by being located to the rear of the mixed-use buildings on the block and setback from all streets by at least 20'. <u>Surface parking can occur where parking structures are allowed.</u>
	Exception	Parking lots are allowed along Carmel Street, Street Type <u>E-1 and J-1</u> and the segment of Street Type I located between Carmel Street and Street Type A-1 , provided they are screened with landscaping.

Driveways

Allowed and Prohibited Locations	Driveway connections to the following street types are prohibited: A1, A2, C, and L. Driveways may connect to all other street types. Shared driveways that provide access to multiple properties are encouraged.
----------------------------------	---



Mixed-Use Building A: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

Facades along the East Urban Plaza~~s~~ and Street Types A1, A2, A3, C, and the segment of E-2 that is within 75' south ~~and 175' north~~ of the intersection of Street Types A-2 and E-2 shall be designed with the following building frontage:

Storefronts

~~Exception: Up to 30% of the northern edge of the East Urban Plaza~~s~~ may be occupied by an anchor retail store frontage.~~

Facades along Street Type L shall be designed with one or more of the following building frontages:

Arcade/Gallery, Storefronts, ~~or Anchor Retail Store Frontage~~

Facades along Street Types E-2¹, ~~F~~, H, and I shall be designed with one or more of the following building frontages:

Storefronts, Live-Work Fronts, or Office Fronts

Facades along Street Types I (segments to the north of Street Types A-1, A-2, and A-3), F, E-2¹, J1 and J2 shall be designed with one or more of the following building frontages:

Storefronts, Live-Work Fronts, Office Fronts, ~~Anchor Retail Store Frontage~~, or Parking Structure Frontages

Standards for the above frontages are provided in Section VI.7 (Frontage and Projection Standards).

¹ Excluding the segment of Street Type E-2 that is within 75' south ~~and 175' north~~ of the intersection of Street Types A-2 and E-2. Storefronts are required in this segment.

Mixed-Use Building A: Parking Requirements**Residential Units and**

Minimum Requirement	1 space per unit
---------------------	------------------

Live Work Units

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
---------------------	--

Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
---------------------	--

Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Mixed-Use Building A: Building Height and Mass**Number of Floors (excluding parking levels)**

Minimum	See Figure VI.C
Maximum	See Figure VI.C

Floor-to-Floor Heights: Mixed-Use Building (excluding parking levels)

A Ground Floor	15' minimum and 22' maximum if along Street Types A-1, A-2, A-3, and C. 12' minimum and 22' maximum along all other street types.
-----------------------	--

Floor-to-Floor Height: Anchor Retail Store (excluding parking levels)

Ground Floor	18' minimum to 30' maximum
Overall Height along	Along the East Urban Plazas, the overall height of the building shall be at least 36'.

Ground Floor

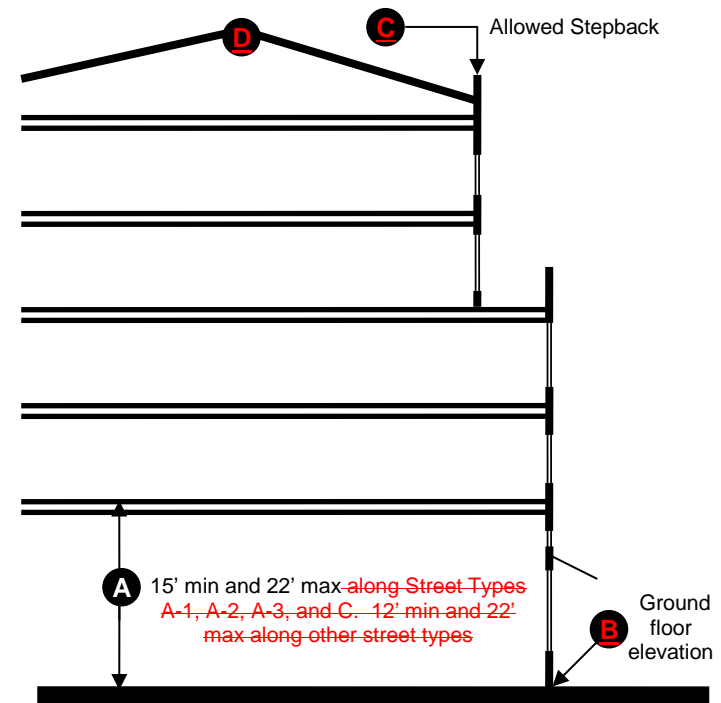
B Elevation	The elevation of the ground floor shall be located near the elevation of the adjacent sidewalk (or plaza) so that external steps and/or ramps are not required to enter the building from the sidewalk.
--------------------	---

Stepbacks

C Allowed and Prohibited	Stepbacks are allowed on the third floor and above, but are not required.
---------------------------------	---

Roofs

D Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.
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Mixed-Use Building A: Building Uses**Upper Floors**

A	Permitted by Right	Retail (2 nd floor), General Service, Health Service, Office, Residential, Live-Work, <u>Student Housing (allowed on the south side of the first block of Main Street eastwest of Campus Way, except along Main Street, and anywhere east of Redel Road)</u>
B	Conditionally Permitted Uses*	Retail (above 2nd floor) (md), Dining (md), Outdoor Dining (md), Dining/Entertainment (M), Entertainment/Recreation (M), Lodging (m), Assembly (M), Cultural/Civic Institutions (M), Alcohol Sales (dM)

Ground Floor with frontage along the East Urban Plaza^s and the following Street Types: A-1, A-2, A-3, and C, and the segment of E-2 that is within 75' south and 175' north of the intersection of Street Types A-2 and E-2 :

	Permitted by Right	Retail, General Service, Dining ¹
C	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (m), Alcohol Sales (dM)

Ground Floor with frontage along Street Types ~~E-1, E-2², F, L, and J-1, and H:~~

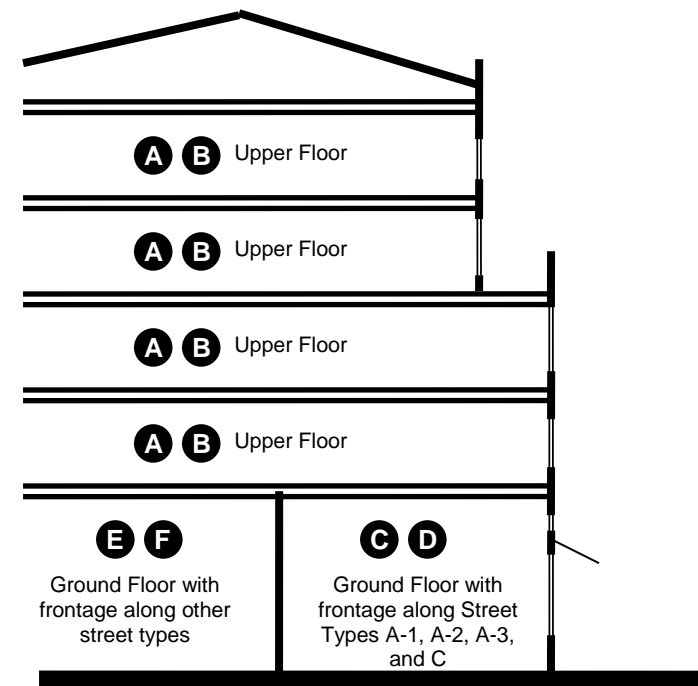
E	Permitted by Right	Retail, General Service, Dining, Office, Live-Work, <u>Residential (except along Main Street, Barham Drive, Discovery Street, and Twin Oaks Valley Road)(d), Student Housing (along Barham Drive, except at the intersection of Barham Drive/Campus Way)</u>
F	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (m), Cultural/Civic Institutions (M), Health Service (m), Alcohol Sales (dM), <u>Temporary Uses(service or educational in nature)(d)</u>

*** Notes:**

(d) Use requires Director's Permit, (m) Use requires Minor Use Permit, (M) Use requires Major Use Permit

1: All Retail, General Service, and Dining Uses that occupy more than 10,000 square feet of ground floor space shall require a Director's Permit.

2: Excluding the segment of Street Type E-2 that is within 75' south and 175' north of the intersection of Street Types A-2 and E-2. Storefronts are required in this segment.



Form-Based Code: VI-g8

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.

Development Standards for Mixed-Use Building B

Description

Mixed-Use Building B: a multi-story building that contains a mix of commercial, office, and residential uses. The ground floor generally contains storefronts, offices, or live-work units. Upper floors generally contain residential units or office space.



Mixed-Use Building B: Lot Size and Building Placement**Block Subdivisions and Lot Size**

The entire development site may be developed as one project or subdivided into smaller properties.

A Lot Width 20' minimum

B Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

All Property Lines A setback is not required.

C Exception An 8' setback is required from external side property lines along Street Type L and K.

Build-to-Line (BTL)

D Build-to-Line Building facades shall be built within 0' to 8' of the front and external side property lines.

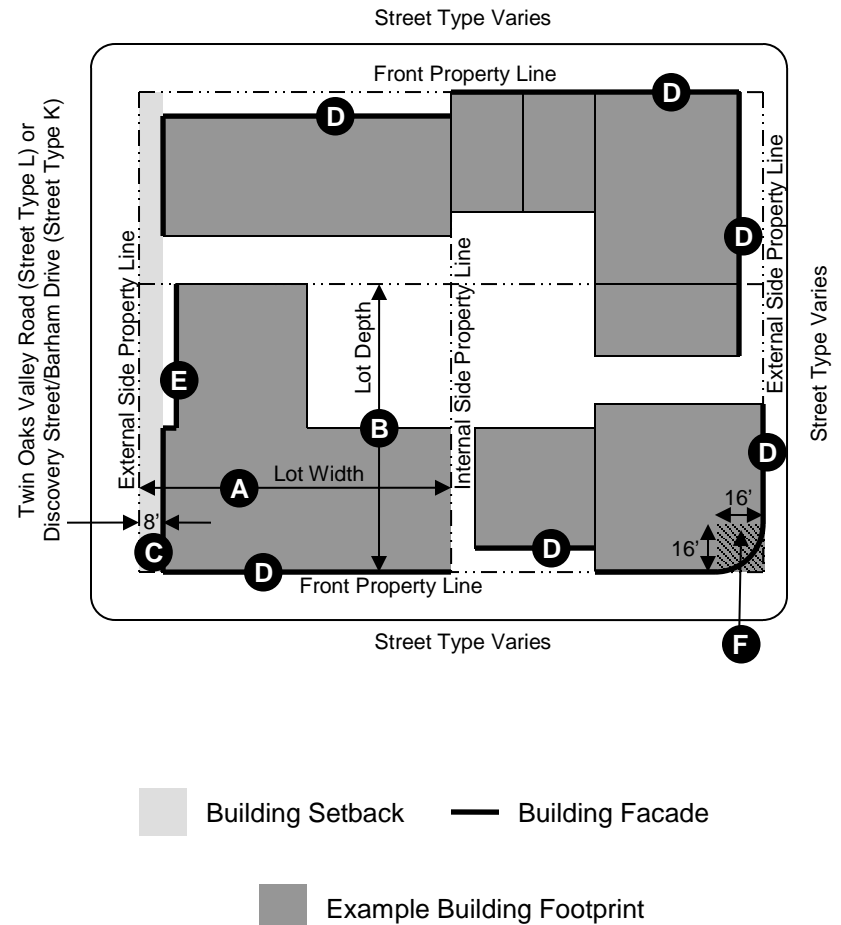
E Exception A Facades shall be built within 8' to 12' of the property lines along Street Type L and K.

F Exception B At block corners, the facade may deviate from the BTL within an area that is not greater than 16' by 16' square.

Exception C For properties adjacent to Twin Oaks Plaza East and West, facades shall be built along the edges of the Plaza(s).

Exception D The BTL may be set back to allow for forecourts.

Frontage Buildout 75% to 100% of the site's total street frontage (as measured by the total length of front and external side property lines) shall be occupied by one or more building facades.



Mixed-Use Building B: Vehicle Access and Parking

Parking Options

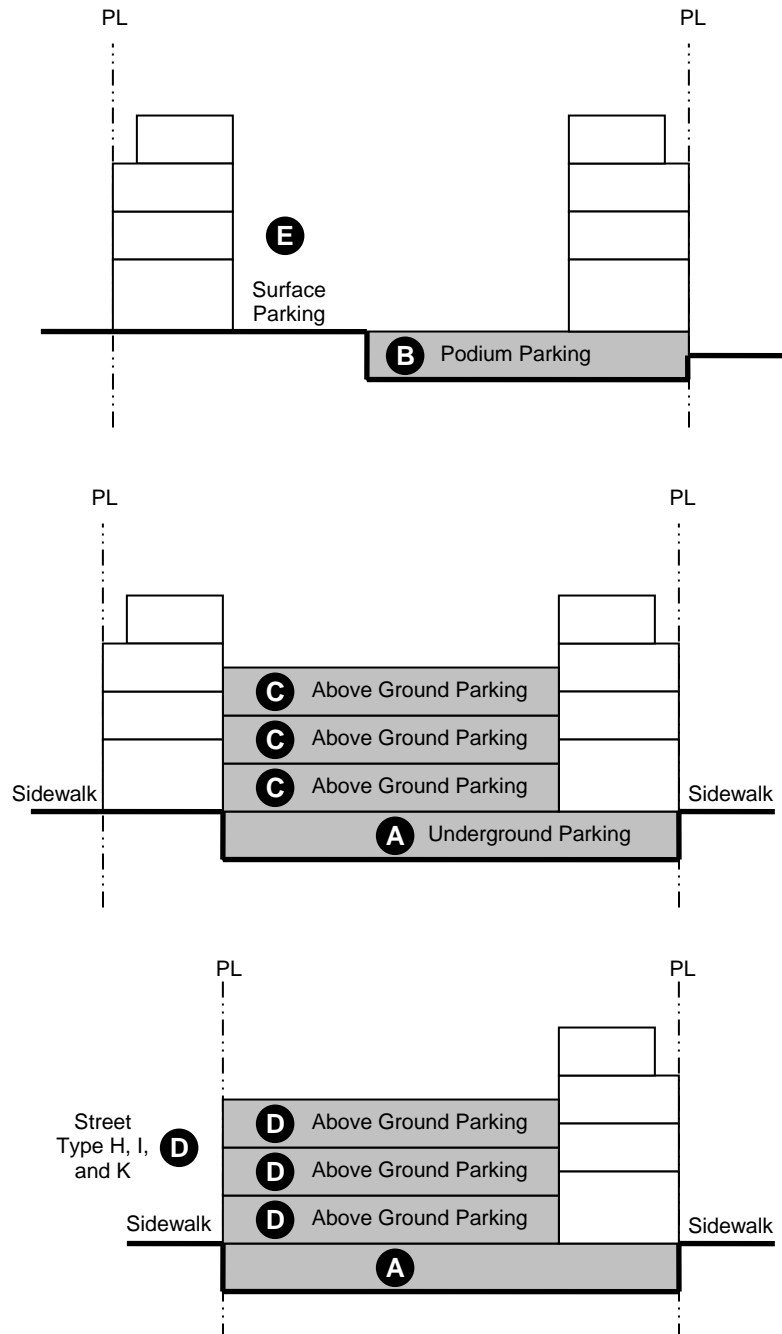
Combinations of parking options are allowed, including underground, podium, and above ground parking structures and surface parking lots.

Parking Locations

A	Underground Parking Levels	Underground parking levels are allowed on the entire site.
B	Podium parking Levels	Podium parking levels are allowed on the entire site, excluding setbacks.
C	Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view by locating them to the rear of all mixed-use buildings on the block.
●	Exception	Parking structures do not have to be screened from public view along Street Types H, I, and K (parking structure facades are allowed along these streets).
E	Surface Parking and Delivery Zones	Surface parking lots and delivery zones are allowed if they are located to the rear of the mixed-use buildings on the block and setback from all streets by at least 20'. <u>Surface parking can occur where parking structures are allowed.</u>

Driveways

Allowed and Prohibited Locations	Driveway connections to the following street types are prohibited: B, K, and L. Driveways may connect to all other street types. Shared driveways that provide access to multiple properties on the block are encouraged.
----------------------------------	---



Mixed-Use Building B: Building Height and Mass**Number of Floors (excluding parking levels)**

Minimum See Figure VI.C

Maximum See Figure VI.C

Floor-to-Floor Heights (excluding parking levels)

- A** Ground Floor 15' minimum and 22' maximum ~~if along Street Type A-3, 12' minimum and 22' maximum along all other street types.~~

Ground Floor

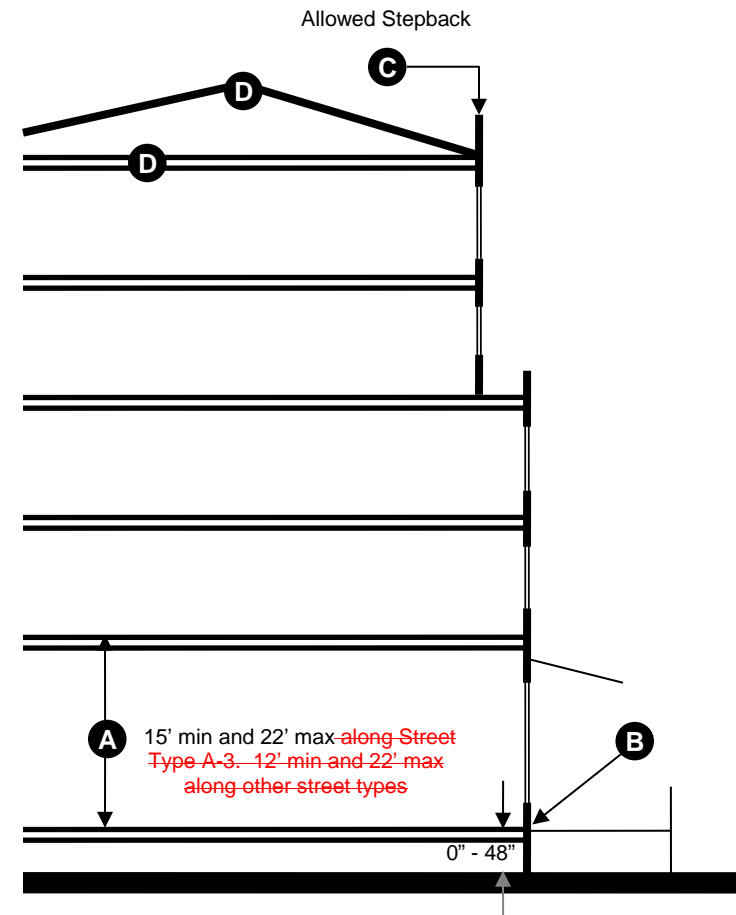
- B** Elevation The elevation of the ground floor shall be located within 0" to 48" of the adjacent sidewalk (or plaza). The elevation of the ground floor where residential uses are located shall be within 18" to 48" above the average grade of the adjacent sidewalk or paseo. If needed, exterior stairs or ramps are allowed to connect the sidewalk to the entrance.

C Stepbacks

Allowed and Prohibited Stepbacks are allowed on all upper floors, but are not required.

D Roofs

Roof Forms and Slope A variety of roof forms are allowed to create diversity and interest.



Mixed-Use Building B: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

Facades along the Twin Oaks Plaza (East) and Twin Oaks Plaza (West) shall be designed with the following building frontage:

Storefronts

Facades along Street Type L and A-3 shall be designed with one or more of the following building frontages:

Storefronts

Arcade/Gallery

Live-Work Fronts

Office Fronts

Facades along Street Types ~~F, E2, D1, A-1~~ and ~~A-2E3~~ shall be designed with one or more of the following building frontages:

Storefronts

Live-Work Fronts

Office Fronts

Stoops

Residential Frontages

Facades along Street Types H, I, and K shall be designed with one or more of the following building frontages:

Storefronts

Live-Work Fronts

Office Fronts

Parking Structure Frontages

Standards for the above frontages are provided in Section VI.7 (Frontage and Projection Standards).

Mixed-Use Building B: Parking Requirements**Residential Units and**

Minimum Requirement	1 space per unit
---------------------	------------------

Live-Work Units

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
---------------------	--

Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
---------------------	--

Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures.

Mixed-Use Building B: Building Uses**Upper Floors**

A	Permitted by Right	Retail (2 nd floor), General Service, Health Service, Office, Residential, Live-Work
B	Conditionally Permitted Uses*	Retail (above 2nd floor) (m), Dining (m), Outdoor Dining (m), Dining/Entertainment (M), Entertainment/Recreation (M), Lodging (m), Assembly (M), Cultural/Civic Institutions (M), Alcohol Sales (dM)

Ground Floor

C	Permitted by Right	Retail, General Service, Health Service, Dining, Office, Live-Work, <u>Residential (except along Main Street east of Twin Oaks Valley Road, Barham Drive, Discovery Street, and Twin Oaks Valley Road)(d), Student Housing (along Barham Drive, except at the intersection of Barham Drive/Campus Way)</u>
D	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (m), Civic/Cultural Institutions, Alcohol Sales (dM), <u>Temporary Uses(service or educational in nature)(d)</u>

*** Notes:**

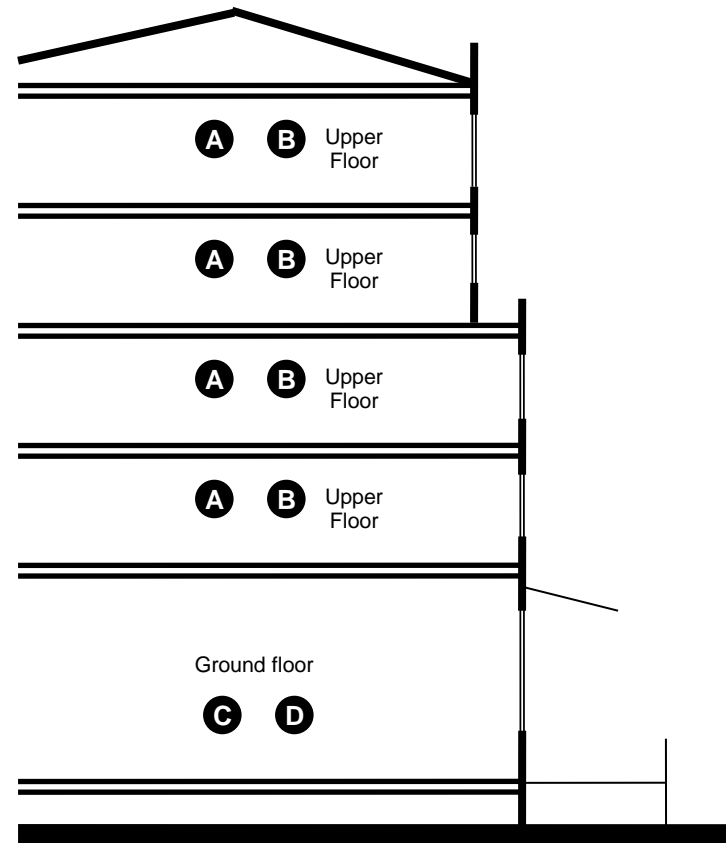
(d) Use requires Director's Permit, (m) Use requires Minor Use Permit, (M) Use requires Major Use Permit

~~(d) Use requires Director's Permit~~

~~(m) Use requires Minor Use Permit~~

~~(M) Use requires Major Use Permit~~

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not



Development Standards for Commercial Building

Description

Commercial Building: a two story commercial building that may contain one or more anchor retail stores and/or smaller storefronts.



Commercial Building: Lot Size and Building Placement

Block Subdivisions and Lot Size

A Lot Width 50' minimum

B Lot Depth 50' minimum

Building Setbacks from Property Lines (PL)

All Property Lines A setback is not required.

C Exception A An 8' setback is required from external side property lines along Street Type L.

Build-to-Line (BTL)

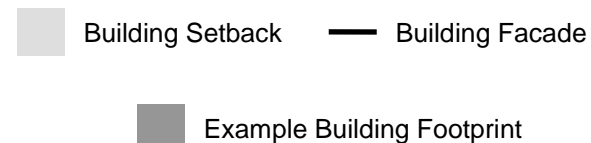
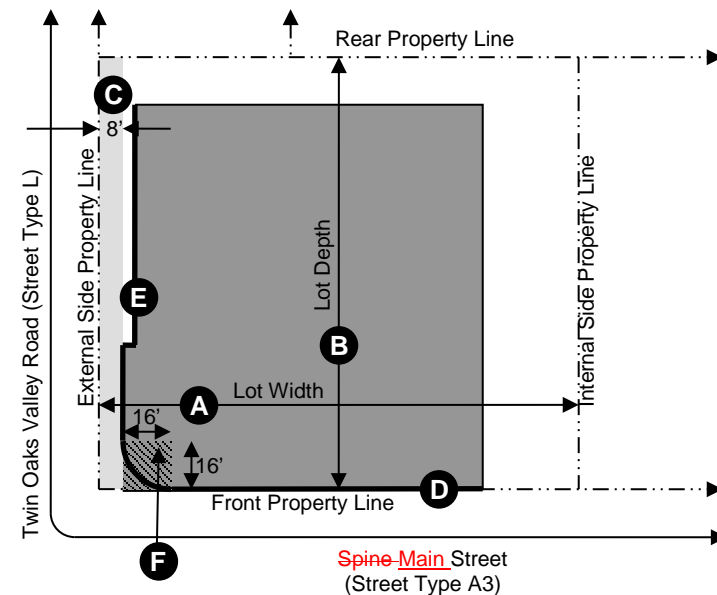
D Build-to-Line Building facades shall be placed along front and external side property lines at a 0' setback.

E Exception A Facades shall be placed within 8' to 12' of the property line along Street Type L.

F Exception B At block corners, the front facade may deviate from the BTL within an area that is not greater than 16' by 16' square.

Exception C The BTL may be set back to allow for forecourts.

Frontage Buildout At least 75% of the site's total street frontage along Street Types ~~L, I, and H~~ (as measured by the total length of front and external side property lines) shall be occupied by one or more building facades. Facades along Street Type A-3 shall have 100% frontage build-out.



Commercial Building: Vehicle Access and Parking

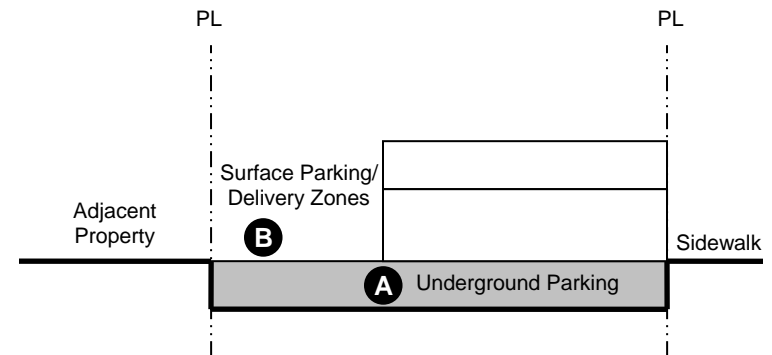
Combinations of parking options are allowed, including above grade structures, underground structures, and surface parking lots.

Parking Locations

A	Underground Parking Levels	Above ground parking levels are allowed if screened from public view.
	Above Ground Parking Levels	Above ground parking levels are prohibited.
B	Surface Parking and Delivery Zones	Surface parking lots and delivery zones are allowed if they are screened from public view by being located to the rear of the mixed-use buildings on the block and setback from all streets by at least 20'. Surface parking and delivery zones are prohibited along Street Types L and A-3.

Driveways

Allowed and Prohibited Locations	Driveway connections to Street Type L are prohibited. Driveways may connect to Street Types H and I. Shared driveways that provide access to multiple properties on the block are encouraged.
----------------------------------	---



Commercial Building: Building Height and Mass**Number of Floors (excluding parking levels)**

A Minimum	1 (if developed with 1-story, the facade shall be designed with windows and other features that reflect the character of a multi-story building.)
Maximum	3

Floor-to-Floor Heights (excluding parking levels)

B Ground Floor	20' minimum to 30' maximum
C Overall Height along	The overall height of the building shall be at least 28'.

Ground Floor

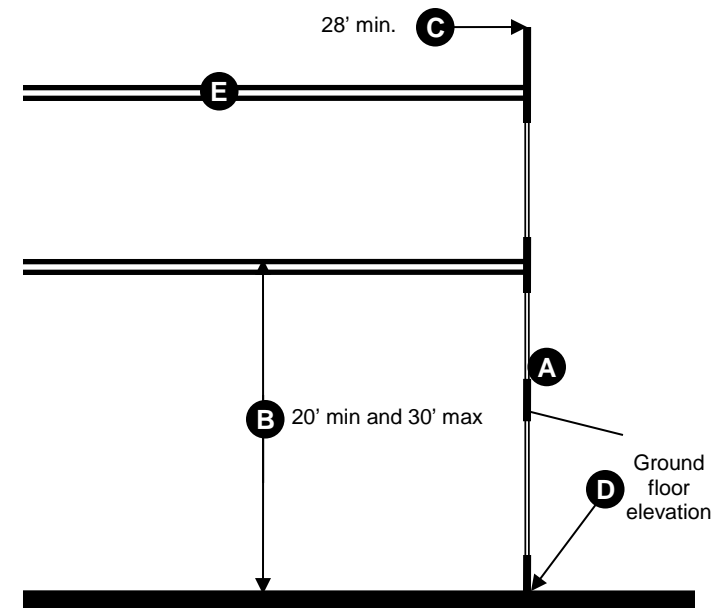
D Elevation	The elevation of the ground floor shall be located near the elevation of the adjacent sidewalk so that external steps and/or ramps are not required to enter the building from the sidewalk.
--------------------	--

Stepbacks

Prohibited	Upper floor stepbacks are prohibited.
------------	---------------------------------------

Roofs

E Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.
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Commercial Building: Frontages

All facades that are adjacent to a street shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

Facades along Street Type L shall be designed with one or more of the following building frontage frontages:

Storefronts

Arcade/Gallery

Facades along Street Type H shall be designed with one or more of the following building frontages:

Storefront

Anchor Retail Store Frontage

Facades along Street Type A-3 shall be designed with one or more of the following building frontages:

Storefront

Standards for the above frontages are provided in Section VI.7 (Frontage and Projection Standards).

Commercial Building A: Parking Requirements**Non-Residential Uses**

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
---------------------	--

Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Commercial Building: Building Uses**Upper Floor**

A	Permitted by Right	Specialty Retail
B	Conditionally Permitted Uses*	Retail (M), Dining (M), General Service (M), Health Service (M), Office (M), Outdoor Dining (M), Dining/Entertainment (M), Entertainment/Recreation (M), Alcohol Sales (dM)

Ground Floor

C	Permitted by Right	Specialty Retail
D	Conditionally Permitted Uses*	Retail (M), General Service (M), Dining (M), Outdoor Dining (M), Dining/Entertainment (M), Alcohol Sales (dM)

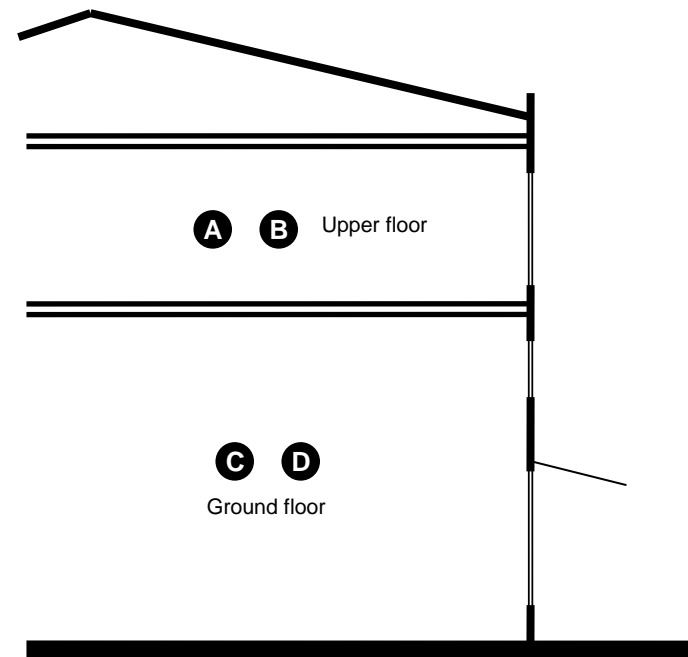
*** Notes:**

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



Development Standards for Freeway Commercial Building A

Description

Freeway Commercial Building A: a multi-story building that is located near Highway 78 along Carmel Street. The building contains commercial and/or office space. Surface parking lots may be located to the rear or sides of the building.



Freeway Commercial Building A: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller properties.

A Lot Width 46' minimum

B Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

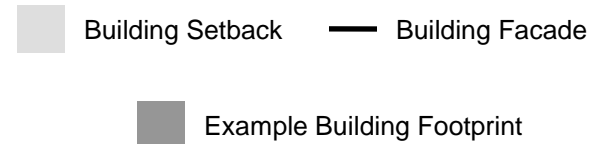
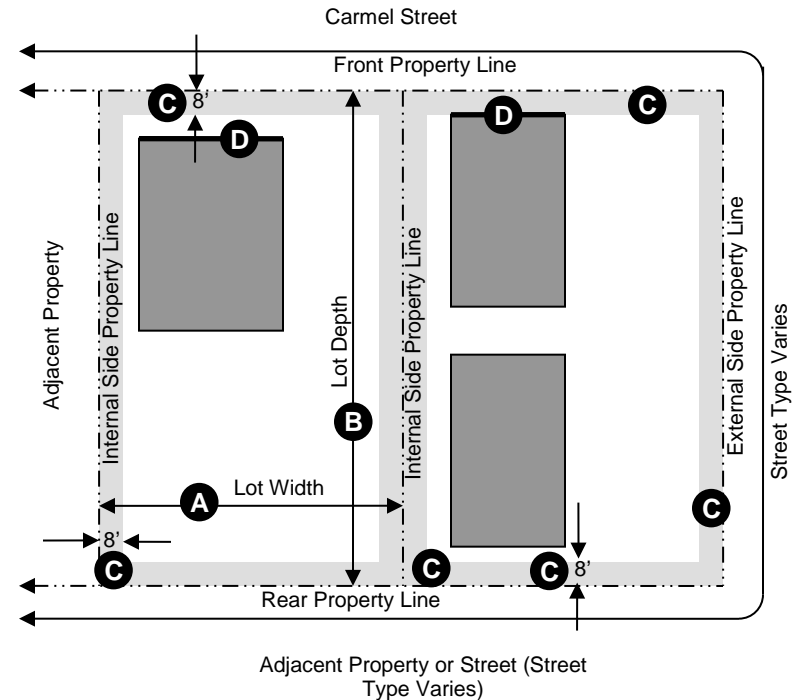
C All Property Lines An 8' setback is required from all property lines.

Build-to-Line (BTL)

D Build-to-Line A facade shall be built within 8' to 16' of the front property line.

Exception A The BTL may be set back to allow for forecourts.

Frontage Requirement At least 25% site's total street frontage along Carmel Street (as measured by the total length of the front property line) shall be occupied by one or more building facades.



Freeway Commercial Building A: Vehicle Access and Parking

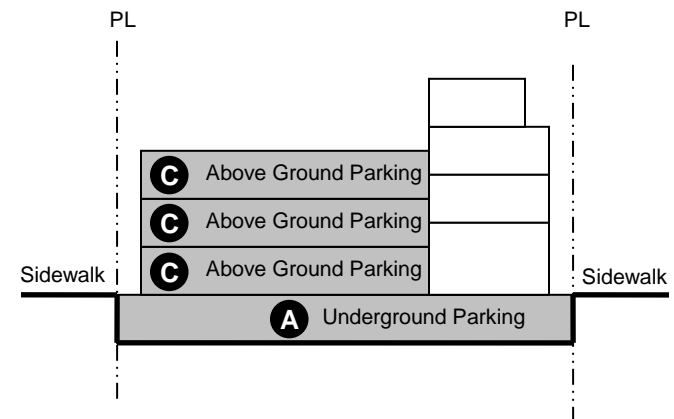
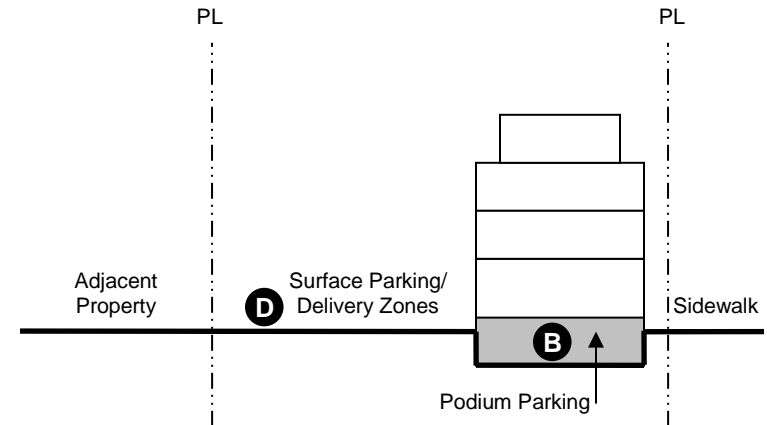
Parking Options

Combinations of parking options are allowed, including underground and above ground parking structures and surface parking lots.

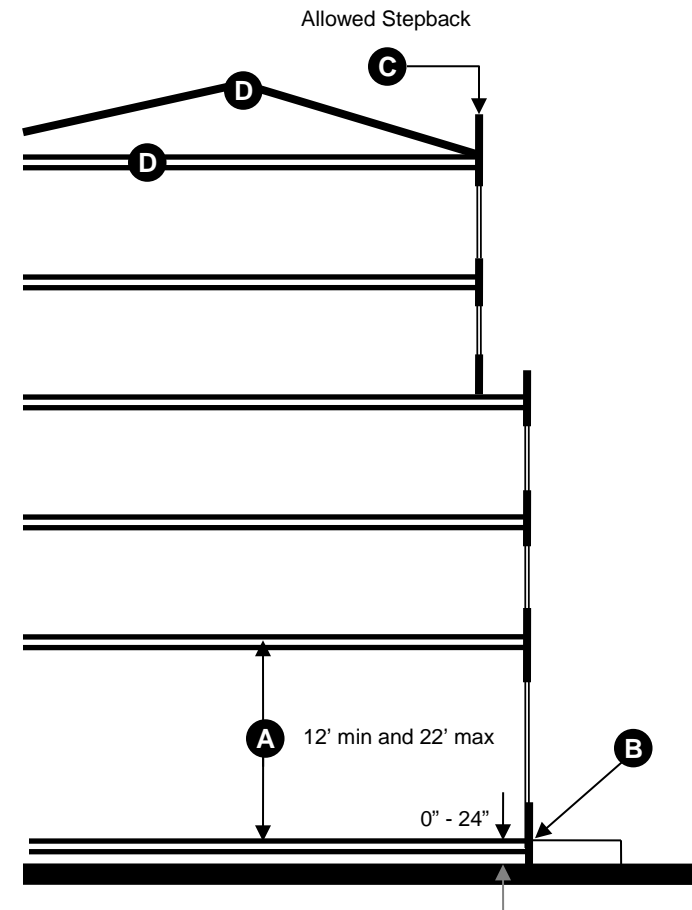
A	Underground Parking Levels	Underground parking structures are allowed on the entire site.
B	Podium parking levels	Podium parking levels are allowed on the entire site, excluding setbacks, if screened from public view.
C	Above Ground Parking Levels	Above ground parking levels are allowed.
D	Surface Parking Lots and Delivery Zones	Surface parking lots and delivery zones are allowed if they are screened from public view by being located to the rear of the building and setback from all streets by at least 8'. Parking is prohibited between the front façade of the building and the sidewalk. <u>Surface parking can occur where parking structures are allowed.</u>

Driveways

Allowed and Prohibited Locations	Driveways may connect to all adjacent street types. Shared driveways that provide access to adjacent properties are encouraged.
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Freeway Commercial Building A: Building Height and Mass		
Number of Floors (excluding parking levels)		
	Minimum	See Figure VI.C
	Maximum	See Figure VI.C
Floor-to-Floor Heights (excluding parking levels)		
A	Ground Floor	12' minimum and 22' maximum
Ground Floor		
B	Elevation	The elevation of the ground floor shall be located 0" to 24" above the elevation of the adjacent sidewalk. If needed, exterior stairs or ramps are allowed to connect the sidewalk to the entrance.
Stepbacks		
C	Allowed and Prohibited	Stepbacks are allowed on all upper floors, but are not required.
Roofs		
D	Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.



Freeway Commercial Building A: Frontages

All facades that are adjacent to a street shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

Facades along all streets (excluding Carmel Street) shall be designed with one or more of the following building frontages:

Storefronts

Office Fronts

Facades along Carmel Street (~~between Street Types E-2 and J-1~~) shall be designed with one or more of the following building frontages:

Storefronts

Office Fronts

Parking Structure Frontages

~~**Facades along Carmel Street (between Street Types E-2 and I) shall be designed with one or more of the following building frontages:**~~

~~Storefronts~~

~~Office Fronts~~

~~Standards for the above frontages are provided in Section VI.7 (Frontage and Projection Standards).~~

Freeway Commercial Building A: Parking Requirements**Non-Residential Uses**

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
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Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Freeway Commercial Building A: Building Uses**Upper Floors**

A	Permitted by Right	Office, Health Service
B	Conditionally Permitted Uses*	Lodging (m), Cultural/Civic Institutions (M), Alcohol Sales (dM)

Ground Floor

C	Permitted by Right	Retail, General Service, Health Service, Dining, Office
D	Conditionally Permitted Uses*	Lodging (m), Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Cultural/Civic Institutions (M), Assembly (M), Alcohol Sales (dM)

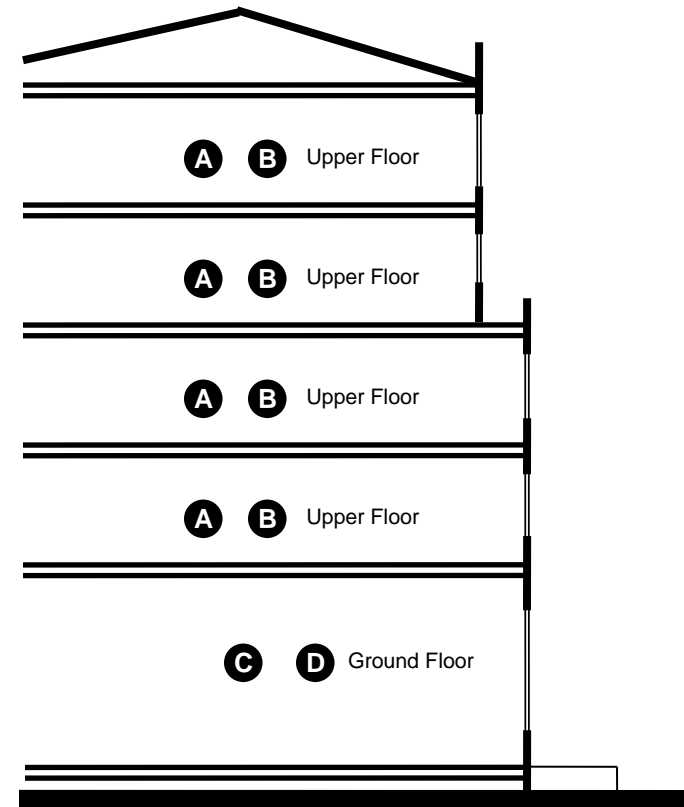
*** Notes:**

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



Development Standards for Freeway Commercial Building B

Description

Freeway Commercial Building B: a multi-story building that is located adjacent to the open space area along State Route 78 west of Twin Oaks Valley Road. The building contains commercial and/or office space. Surface parking lots may be located to the rear or sides of the building.



Freeway Commercial Building B: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller properties.

A Lot Width 100' minimum

B Lot Depth 200' minimum

Building Setbacks from Property Lines (PL)

C Front and Side PL An 8' setback is required from front and side (internal and external) property lines.

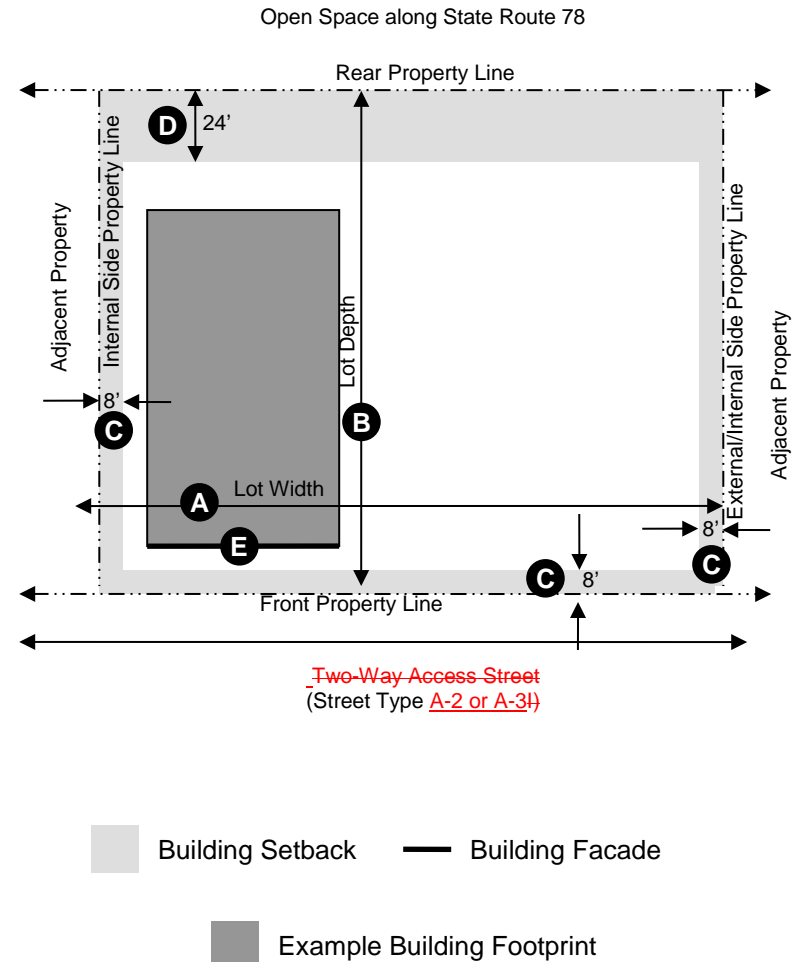
D Rear PL A 24' setback is required from rear property lines.

Build-to-Line (BTL)

E Build-to-Line A facade shall be built within 8' to 16' of the front property line.

Exception The BTL may be set back to allow for forecourts.

Frontage Buildout At least 40% site's total street frontage along Street Type A-2 or A-3 (as measured by the total length of front property lines) shall be occupied by one or more building facades.



Freeway Commercial Building B: Vehicle Access and Parking

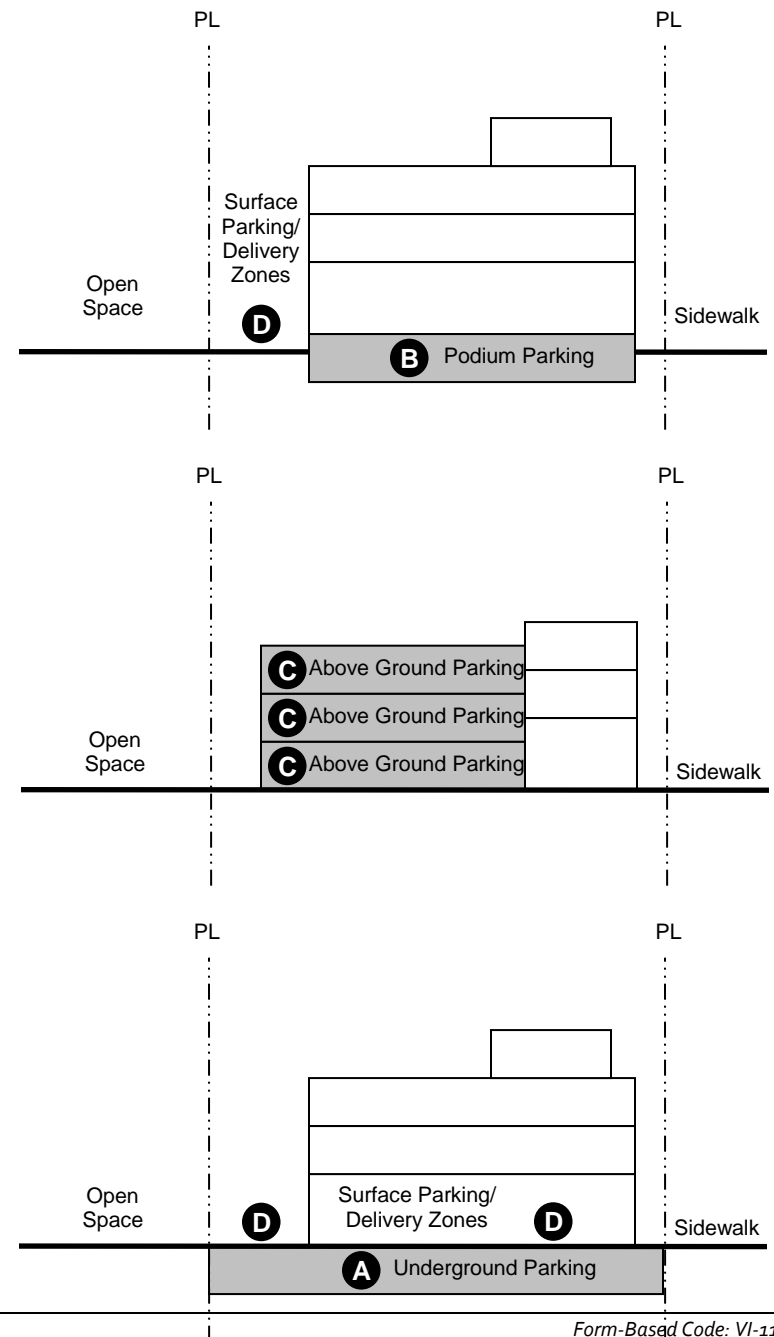
Parking Options

Combinations of parking options are allowed, including above grade structures, underground structures, and surface parking lots.

A	Underground Parking Levels	Underground parking levels are allowed on the entire site.
B	Podium Parking Levels	Podium parking levels are allowed are beneath the building footprint.
C	Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view from Street Type <u>A-2 or A-3</u> by locating them to the rear of the commercial building. Parking structure frontages may be visible from State Route 78.
D	Surface Parking Lots and Delivery Zones	Surface parking lots and delivery zones are allowed if they are setback at least 20 feet from the front property line and are located to the side or rear of the building. Surface parking lots and delivery zones are prohibited between the building and Street Type <u>A-2 or A-3</u> . <u>Surface parking can occur where parking structures are allowed.</u>

Driveways

Allowed and Prohibited Locations	Driveway connections to Street Type L are prohibited. Driveways may connect to Street Type <u>A-2 or A-3</u> . Shared driveways that provide access to adjacent properties are encouraged.
----------------------------------	--



Freeway Commercial Building B: Building Height and Mass**Number of Floors**

Minimum	See Figure VI.C
Maximum	See Figure VI.C

Floor-to-Floor Heights

A Ground Floor	12' minimum 22' maximum
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Ground Floor

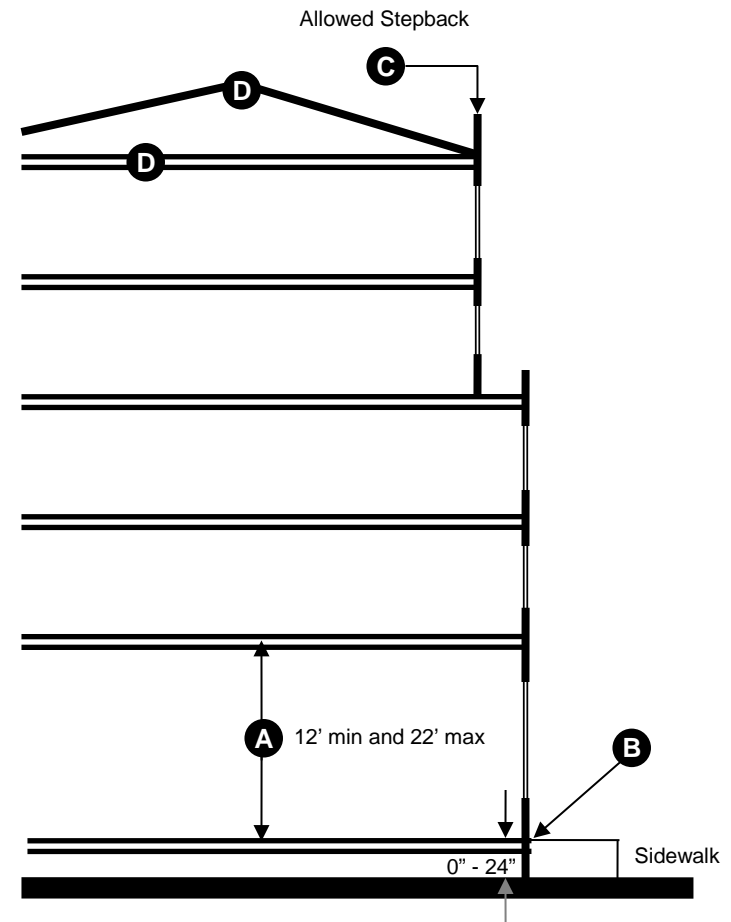
B Elevation	The elevation of the ground floor shall be located 0" to 24" above the elevation of the adjacent sidewalk. Exterior or interior stairs or ramps are allowed to connect the sidewalk to the entrance.
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Stepbacks

C Allowed and Prohibited	Stepbacks are allowed on all upper floors, but are not required.
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Roofs

D Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.
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Freeway Commercial Building B: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

All building facades shall be designed with one or more of the following building frontages:

Office Fronts

Storefronts

Standards for the above frontages are provided in Section VI.7 (Frontage and Projection Standards).

Freeway Commercial Building B: Parking Requirements**Non-Residential Uses**

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
---------------------	--

Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Freeway Commercial Building B: Building Uses**Upper Floors**

A	Permitted by Right	Office, General Service, Health Service
B	Conditionally Permitted Uses*	Lodging (m), Cultural/Civic Institutions (M), Alcohol Sales (dM)

Ground Floor

C	Permitted by Right	Retail, General Service, Health Service, Dining, Office
D	Conditionally Permitted Uses*	Lodging (m), Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Cultural/Civic Institutions (M), Assembly (M), Alcohol Sales (dM)

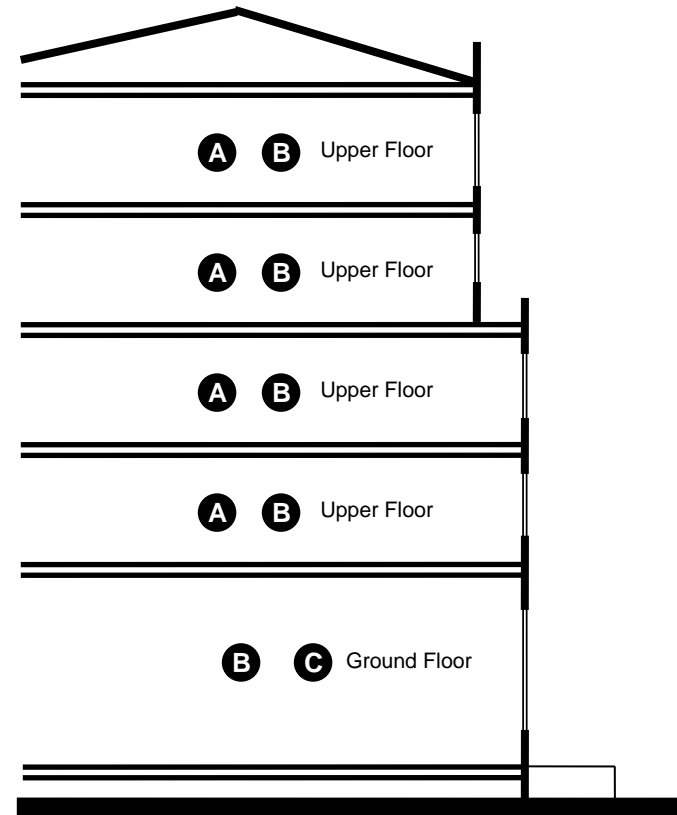
*** Notes:**

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



Development Standards for University Flats

Description

University Flats: Residential units within a multi-story building that includes forecourts and a central courtyard. The residential units may be apartments, condominiums, or student housing. Retail, live-work, or office space may be provided on the ground floor of the building.



University Flats: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller properties.

A Lot Width 50' minimum

B Lot Depth 100' minimum

Building Setbacks from Property Lines (PL)

C Front PL An 8' setback is required from front property lines along Street Type K.

Side and Rear PL A setback is not required.

Build-to-Line (BTL)

D Build-to-Line Building facades shall be placed within 8' to 16' of front property lines and 0' to 8' of external side property lines.

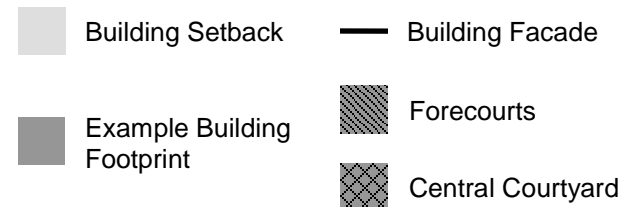
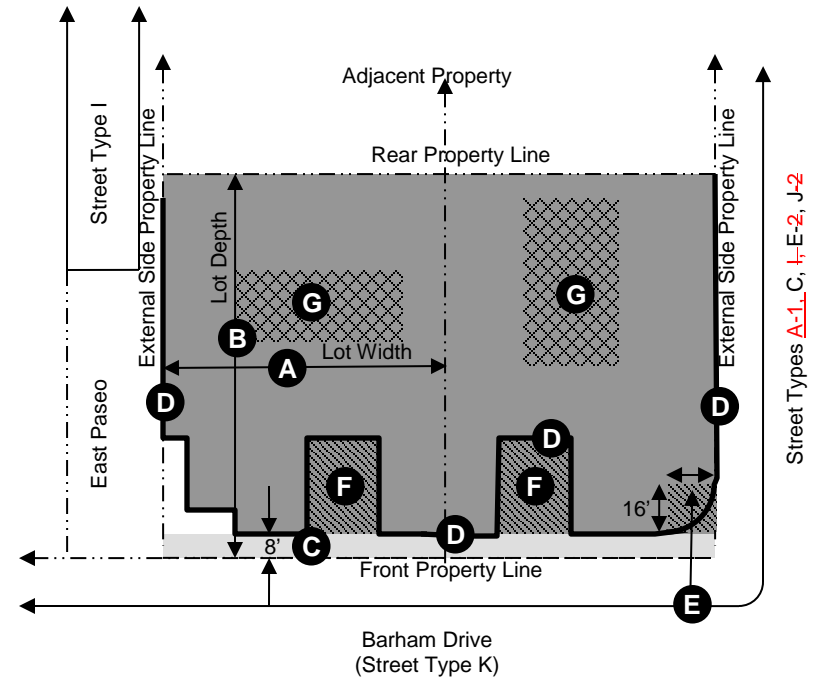
E Exception A At block and paseo corners, the front façade may deviate from the BTL to create architecturally unique corner conditions.

F Exception B The front façade may deviate from the BTL to create forecourts.

Exception C Along Street Type C, the facade shall be built along the property line at a 0' setback.

Frontage Buildout At least 80% of the site's total street and public space frontage (as measured by the total length of front and external side property lines) shall be occupied by one of more building facades.

G Forecourts and Courtyards Common open space shall be provided in the form of forecourts and central courtyards. The total amount of common open space shall equal at least 15% of the site area.



University Flats: Vehicle Access and Parking

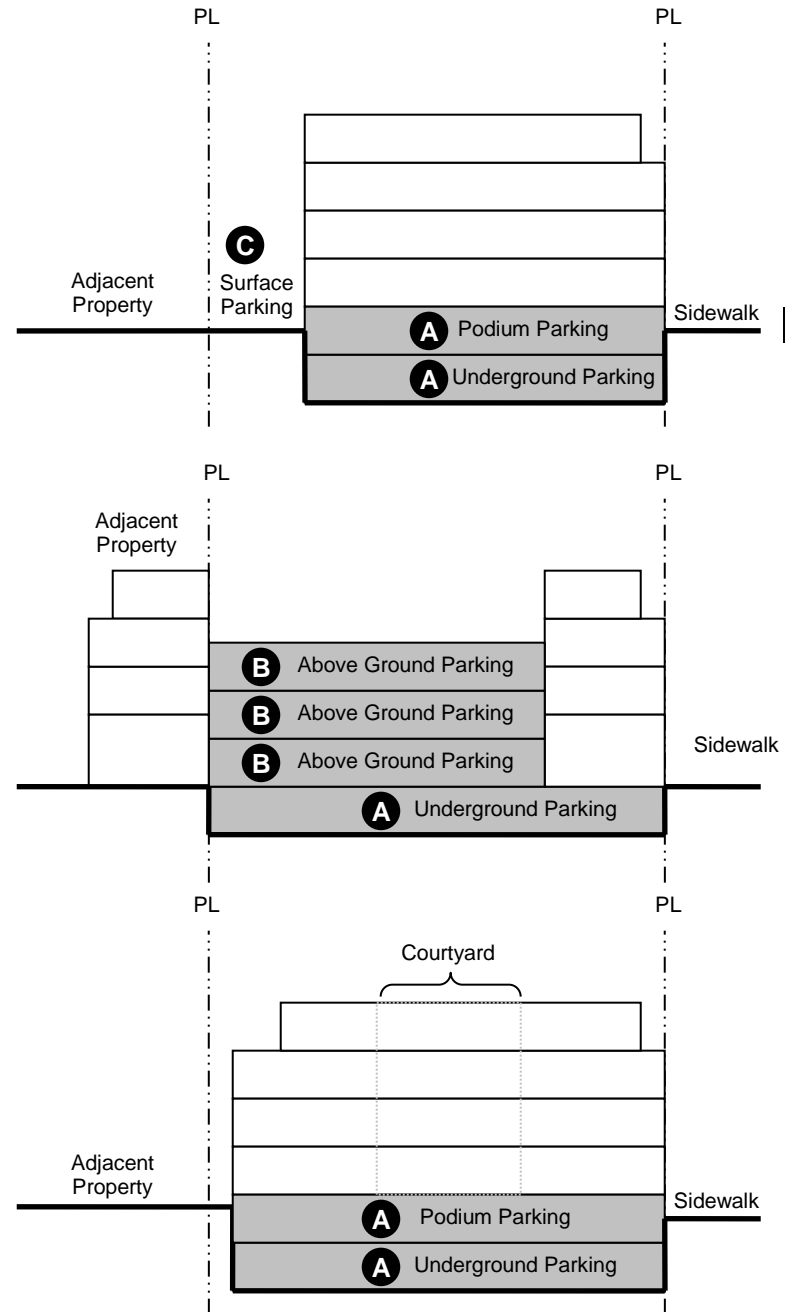
Parking Options

Combinations of parking options are allowed, including underground and podium parking structures and surface parking lots.

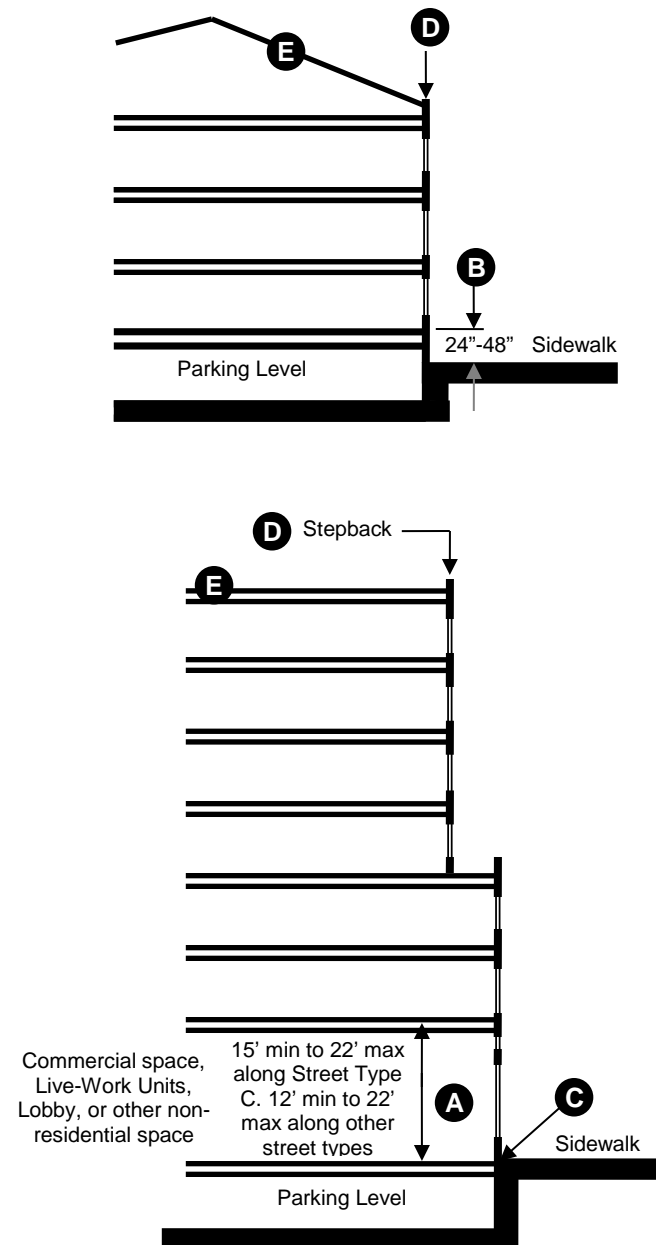
	Underground Parking Levels	Underground parking levels are allowed on the entire site.
A	Podium Parking Levels	Podium parking levels are allowed beneath the building and courtyard.
B	Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view by locating them to the rear of buildings on the block.
C		
D	Surface Parking	Surface parking lots are allowed if they are screened from public view by locating them to the rear of the buildings and setback from all streets and paseos by at least 20'. <u>Surface parking can occur where parking structures are allowed.</u>

Driveways

E	Allowed and Prohibited Locations	Driveway connections to the following street types are prohibited: C and K. Driveways may connect to all other street types. Shared driveways that provide access to multiple properties on the block are encouraged.
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University Flats: Building Heights and Mass		
Number of Floors		
	Minimum	See Figure VI.C
	Maximum	See Figure VI.C
Floor-to-Floor Heights (excluding parking levels)		
A	Ground Floor	15' minimum and 22' maximum if along Street Type C. If the building is designed with commercial space or live-work units on the ground floor along other streets, the floor-to-floor height of the ground floor shall be 15' minimum and 22' maximum.
Ground Floor		
B	Elevation	The elevation of the ground floor where residential uses are located shall be within 18" to 48" above the average grade of the adjacent sidewalk or paseo.
C	Exception A	If the building is designed with commercial space or live-work units on the ground floor, the elevation of the ground floor shall be located near the elevation of the sidewalk so that external steps and/or ramps are not required to enter the building from the sidewalk.
Stepbacks		
D	Allowed and Prohibited	Stepbacks are allowed on all upper floors, but are not required.
Roofs		
E	Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.



University Flats: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, the types of frontages that are allowed vary based on the adjacent street type and the adjacent public space.

Facades along Street Type C shall be designed with the following building frontage:

Storefronts

Facades along Street Type K shall be designed with one or more of the following building frontage:

Residential Frontage

Stoop Frontage

Live-Work Fronts

Office Fronts

Facades along the East Paseos and Street Types ~~E-2, H, IA-1, E-1~~, and ~~J-2~~ shall be designed with one or more of the following building frontages:

Residential Frontage

Stoop Frontage

Storefronts

Live-Work Fronts

Office Fronts

Standards for the above frontages are provided in Section VI.7 (Frontages and Projections).

University Flats: Parking Requirements**Residential Units and**

Minimum Requirement	1 space per unit
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Live Work Units

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
---------------------	--

Student Housing

Minimum Requirement	Parking for students is not required within the <i>University District</i> . Students living within student housing may park their cars at the University.
---------------------	--

Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
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Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

University Flats: Building Uses

Upper Floors

A	Permitted by Right	Student Housing, <u>Residential</u>
	Exception	Where a public pedestrian bridge lands at an upper floor of the building, retail, general service, and dining uses would be allowed on that upper floor.

B	Conditionally Permitted Uses*	Residential (M), Office (M), Lodging (M), Alcohol Sales (<u>dM</u>)
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Ground Floor (with frontage along Street Type C, including the building corners at the intersection of Street Types C and K)

C	Permitted by Right	Retail, General Service, Dining
D	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (M), Alcohol Sales (<u>dM</u>)

Ground floor with frontage along other streets and paseos

E	Permitted by Right	Retail, General Service, Dining, Office, Live-Work, Student Housing
F	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (M), Cultural/Civic Institutions (M), Residential (M), Alcohol Sales (<u>dM</u>)

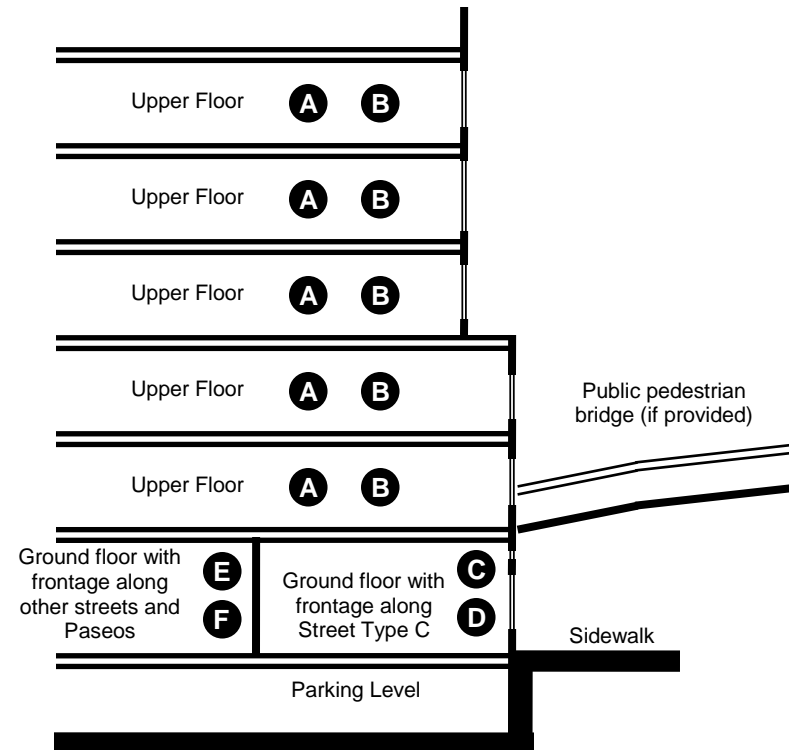
* Notes:

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



Development Standards for Office Flats

Description

Office Flats: A series of multi-story buildings that generally contain office uses, but may also contain retail space on the ground floor of the building.



Office Flats: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller projects.

A Lot Width 40' minimum

B Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

All Property Lines A setback is not required.

C Exception An 8' setback is required from Street Type K.

Build-to-Line (BTL)

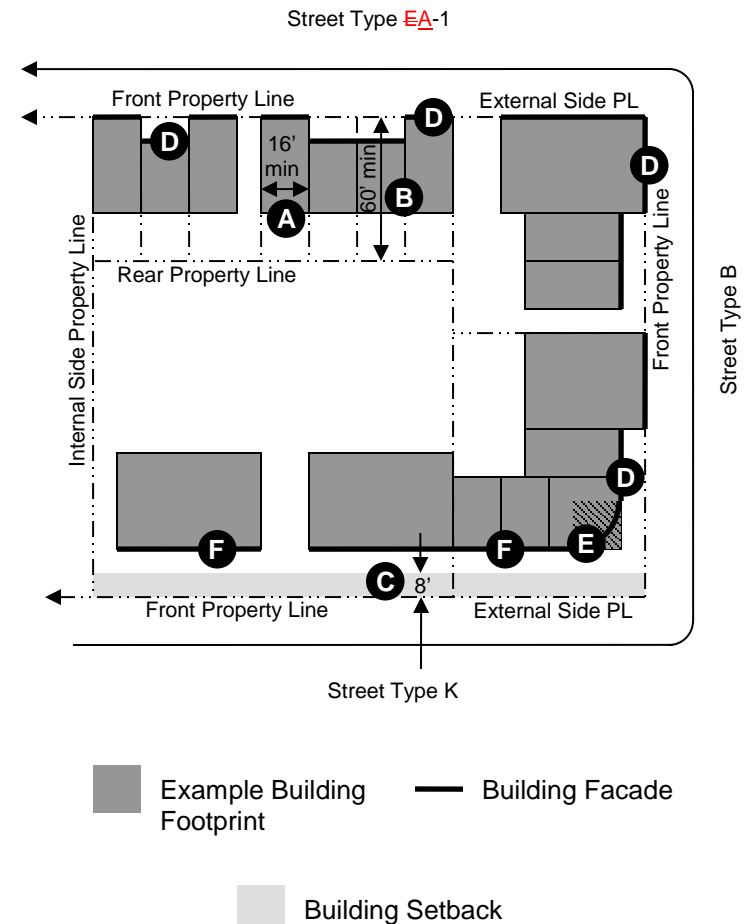
D Build-to-Line Facades shall be built within 0' to 8' of the front and external side property lines.

E Exception A At block corners, the front facade may deviate from the BTL to create architecturally unique corner conditions

F Exception B Facades shall be built within 8' to 24' of front and external side property lines along Street Type K.

Exception C The BTL may be set back to allow for forecourts.

Frontage Buildout At least 75% of the site's street frontage along Street Types B and ~~E+A-1~~ (as measured by the total length of front and external side property lines) shall be occupied by one or more building facades.



Office Flats: Vehicle Access and Parking

Parking Options

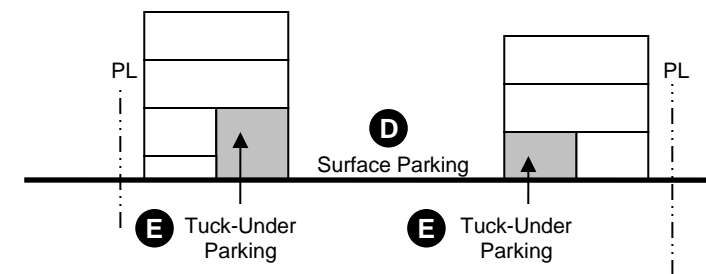
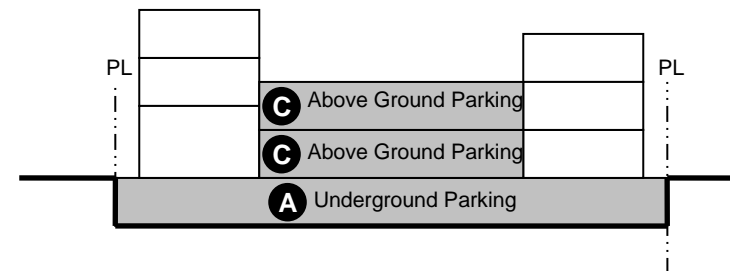
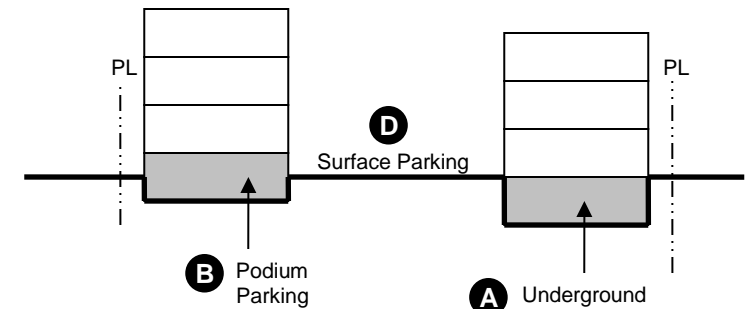
Combinations of parking options are allowed, including underground and podium parking structures and surface parking lots.

A Underground Parking Levels	Underground parking levels are allowed on the entire site.
B Podium Parking Levels	Podium parking levels are allowed beneath the building footprints.
C Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view by locating them to the rear of all buildings on the block.
D Surface Parking and Delivery Zones	Surface parking lots and Delivery Zones are allowed if they are located behind the buildings on the site. Surface parking lots are allowed along Grand Avenue and the West Creek Park, but are prohibited along other streets. Surface parking can occur where parking structures are allowed.
E Tuck Under Parking	Tuck-under parking facilities are allowed if they are screened from public view by locating them to the rear of the buildings. Access to tuck-under parking is only allowed along the rear facade.

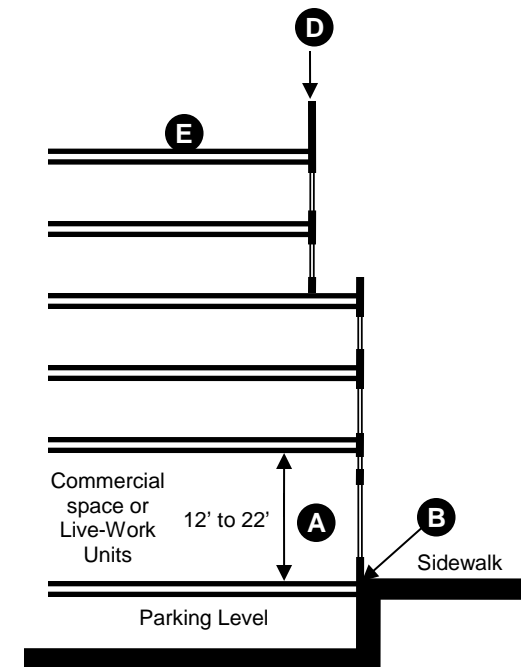
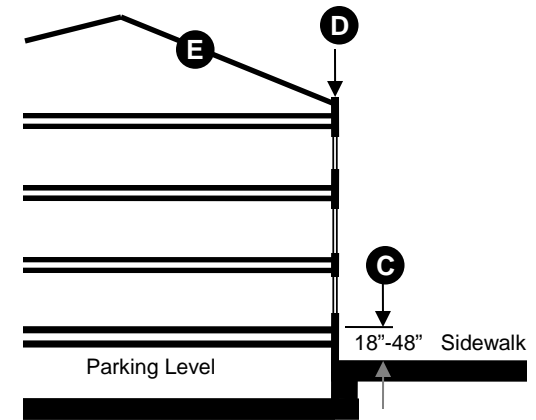
Driveways

Allowed and Prohibited Locations

Driveways may connect to Street Types [EA-1](#) and Grand Avenue. Driveway connections to Street Type B and K are prohibited. Shared driveways that provide access to multiple properties on the block are encouraged.



Office Flats: Building Heights and Mass		
Number of Floors		
Minimum	See Figure VI.C	
Maximum	See Figure VI.C	
Floor-to-Floor Heights (excluding parking levels)		
A Ground Floor	If the building is designed with retail space or live-work units on the ground floor, the floor-to-floor height of the ground floor shall be 12' to 22'.	
Ground Floor		
B Elevation	The elevation of the ground floor shall be within 0" to 48" above the grade of the adjacent sidewalk.	
C Exception	The elevation of the ground floor where residential uses are located shall be within 18" to 48" above the average grade of the adjacent sidewalk or paseo.	
Stepbacks		
D Allowed and Prohibited	Stepbacks are allowed on all floors, but are not required.	
Roofs		
E Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.	



Office Flats: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, a variety of frontages are allowed.

Facades along street types B and K shall be designed with one or more of the following building frontage:

Storefronts

Office fronts

Live-work fronts

Facades along Street Type ~~EA~~-1 shall be designed with one or more of the following building frontage:

Storefronts

Live-work fronts

Stoops

Residential Fronts

Standards for the above frontages are provided in Section VI.7 (Frontages and Projections).

Office Flats: Parking Requirements**Live Work Units**

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
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Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
---------------------	--

Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Office Flats: Building Uses**Upper Floors with Frontage along Street Types B and K**

A	Permitted by Right	Office, Live Work, General Service, Health Service
B	Conditionally Permitted Uses*	Not applicable

Ground Floor with frontage along Street Types B and K

C	Permitted by Right	Retail, General Service, Health Service
B		Dining, Office, Live-Work
D	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (m), Cultural/Civic Institutions (M), Alcohol Sales (dM)
C		

Upper Floors with Frontage along Street Type ~~EA~~-1

E	Permitted by Right	Residential, Office, Live Work, General Service, Health Service
D		
F	Conditionally Permitted Uses*	Lodging (m), Cultural/Civic Institutions (M)

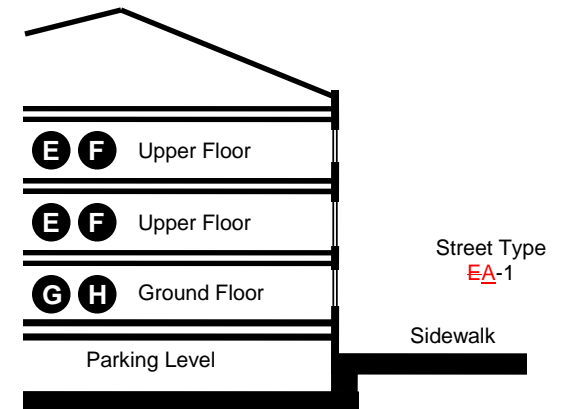
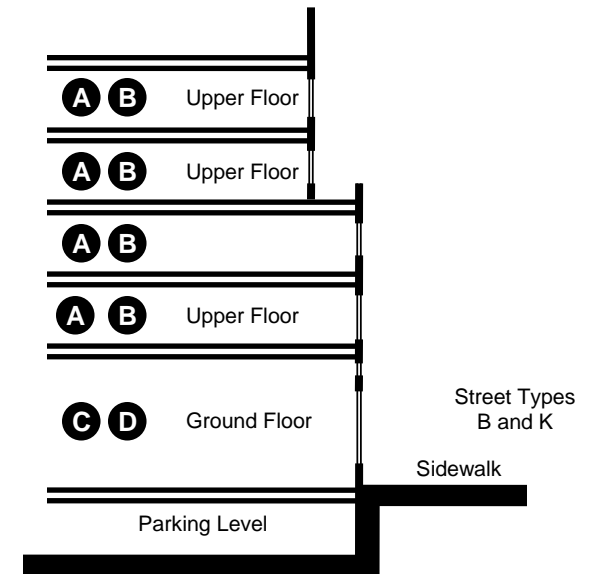
Ground Floor with frontage along Street Type ~~EA~~-1

G	Permitted by Right	Residential, Retail, General Service, Health Service, Dining, Office, Live-Work
L		
H	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Entertainment/Recreation (M), Lodging (m), Cultural/Civic Institutions (M), Alcohol Sales (dM)

***Notes:**

- F**
- (d) Use requires Director's Permit
 - (m) Use requires Minor Use Permit
 - (M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



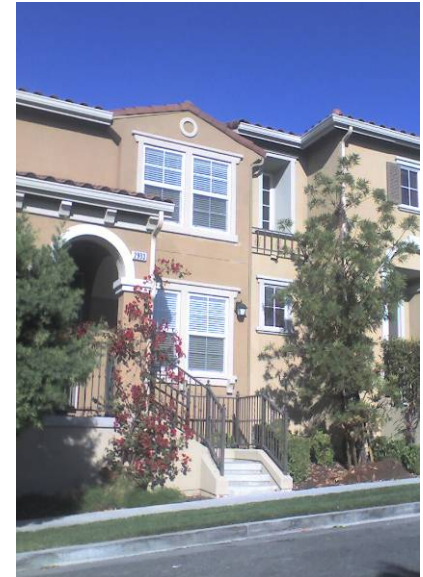
Development Standards for Townhomes/Flats

Description:

Townhomes are multi-story residential units that are placed side-by-side and share side property lines with adjacent units.

Flats are residential units that are stacked vertically to create a ~~multi-multi~~ story building. Both townhomes and flats may be combined within one development. Building heights for townhouses and flats generally range from two to five stories.

Along Street Type B (Residential Avenue), the ground floor of flats may contain commercial space and/or live-work units.



Townhomes/Flats: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire block may be developed as one project or subdivided into smaller properties. If subdivided, a shared driveway system is required to provide access to the parking facilities of each property on the block. The shared driveway system shall be designed to achieve the applicable Vehicle Access and Parking standards on the following page.

A Lot Width 16' minimum

B Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

C Front and External Side PL 5'

D Exception A A setback from the property line along Street Type B is not required.

Internal Side and Rear PL A setback is not required.

Build-to-Line (BTL)

E Build-to-Line Building facades shall be placed within 5' to 12' of front property lines (and external side property lines for corner lots).

F Exception A Building facades shall be placed within 0' to 5' of the property line along Street Type B.

Exception B The BTL may be set back to allow for forecourts.

Exception C Where topography does not allow for BTLs within ranges noted above, the applicant shall confer with the Planning Director to decide upon the most appropriate design measures. Such measures should both maximize the site's functional connectivity to the rest of the project while mitigating excessive topographic elevation changes and grading cost constraints. Any deviations from the above BTL requirements are subject to a Director's Permit.

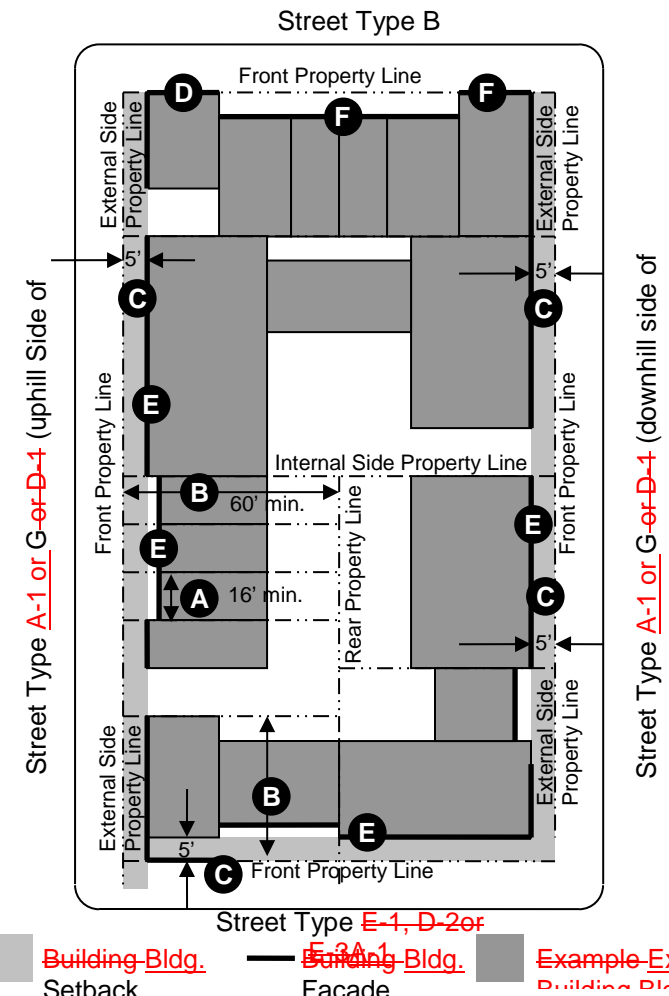
Frontage Buildout

PC HEARING DRAFT 04-28-2014

90% of the total length of the site's total street frontage (the total length of front and external side property lines) shall be occupied by one or more building facades.

Exception A The frontage buildout requirement may be reduced by an additional 24' where driveway access is provided.

Exception B Pursuant to Exception C for the Build-to-Line, any deviations from the above frontage requirements are subject to a Director's Permit.



Townhomes/Flats: Vehicle Access and Parking

Parking Options

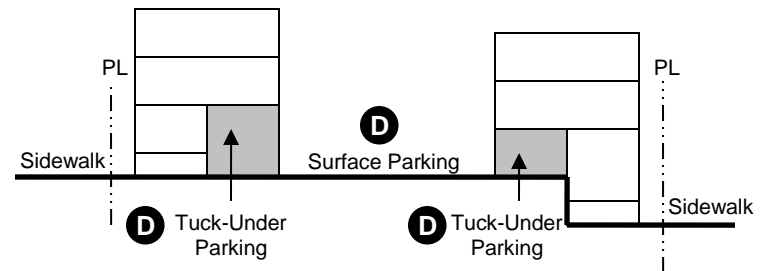
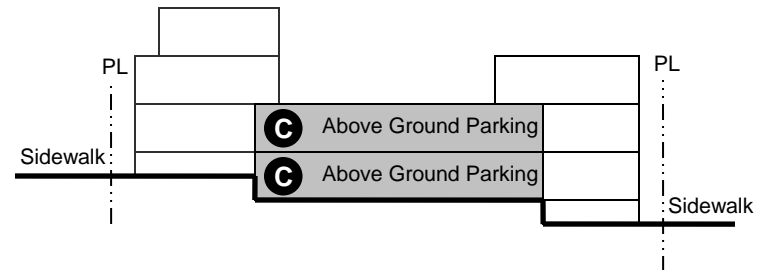
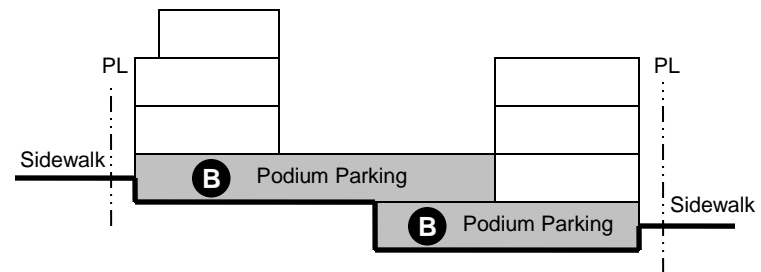
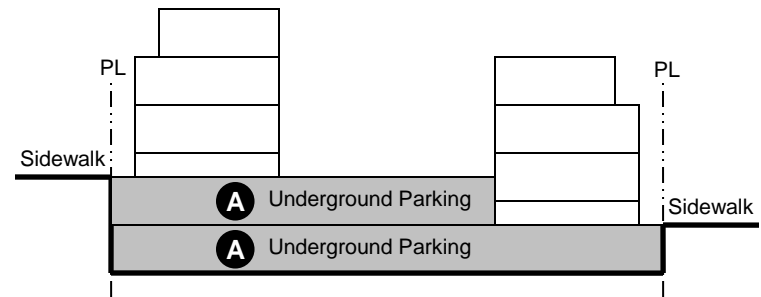
Combinations of parking options are allowed, including underground, podium, surface, and tuck-under parking. Shared parking facilities are also allowed to provide parking for multiple properties on the block.

Parking Locations

A	Underground Parking Levels	Underground parking levels are allowed on the entire site.
B	Podium Parking Levels	Podium parking levels are allowed within all areas of the site, excluding setbacks.
C	Above Ground Parking Levels	Above ground parking levels are allowed if they are screened from public view by locating them to the rear of the townhomes/flats on the block.
D	Surface and Tuck-Under Parking	Surface parking lots and tuck-under parking facilities are allowed if they are screened from public view by locating them to the rear of the townhomes/flats on the block. Access to tuck-under parking is only allowed along the rear facade.

Driveways

Allowed and Prohibited Locations	Driveways are only allowed to connect to Street Types A-1 and G-and-D+ . If the block is subdivided into multiple lots, a shared driveway system shall be created to provide access to the parking facilities for the each lot. Driveway access point should be limited to 5 per block to reduce the number of curb-cuts and pedestrian/vehicle conflicts along the street.
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Townhomes/Flats: Building Heights and Mass

Number of Floors

Minimum	See Figure VI.C
Maximum	See Figure VI.C

Floor-to-Floor Heights (excluding parking levels)

A Ground Floor	Along Street Type B, the ground floor of flats may contain commercial space and/or live-work units. If the building is designed with commercial space or live-work units on the ground floor, the floor-to-floor height of the ground floor shall be 12' to 22'.
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Ground Floor

B Elevation	The elevation of the ground floor where residential uses are located shall be within 18" to 48" above the average grade of the adjacent sidewalk or paseo.
C Exception <u>A</u>	If the building is designed with commercial space or live-work units on the ground floor, the elevation of the ground floor shall be located within 0" to 48" of the adjacent sidewalk.

Exception B Where topography does not allow for ground floor elevations within the ranges noted above, the applicant shall confer with the Planning Director to decide upon the most appropriate design measures. Such measures should both maximize the site's functional connectivity to the rest of the project while mitigating excessive topographic elevation changes and grading cost constraints. Any deviations from the above ground floor elevation requirements are subject to a Director's Permit.

Stepbacks

D Allowed and Prohibited	Stepbacks are allowed on all floors, but are not required.
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Roofs

Form-Based Code: VI-138

~~Roof Forms and Slope~~

~~A variety of roof forms are allowed to create diversity and interest.~~

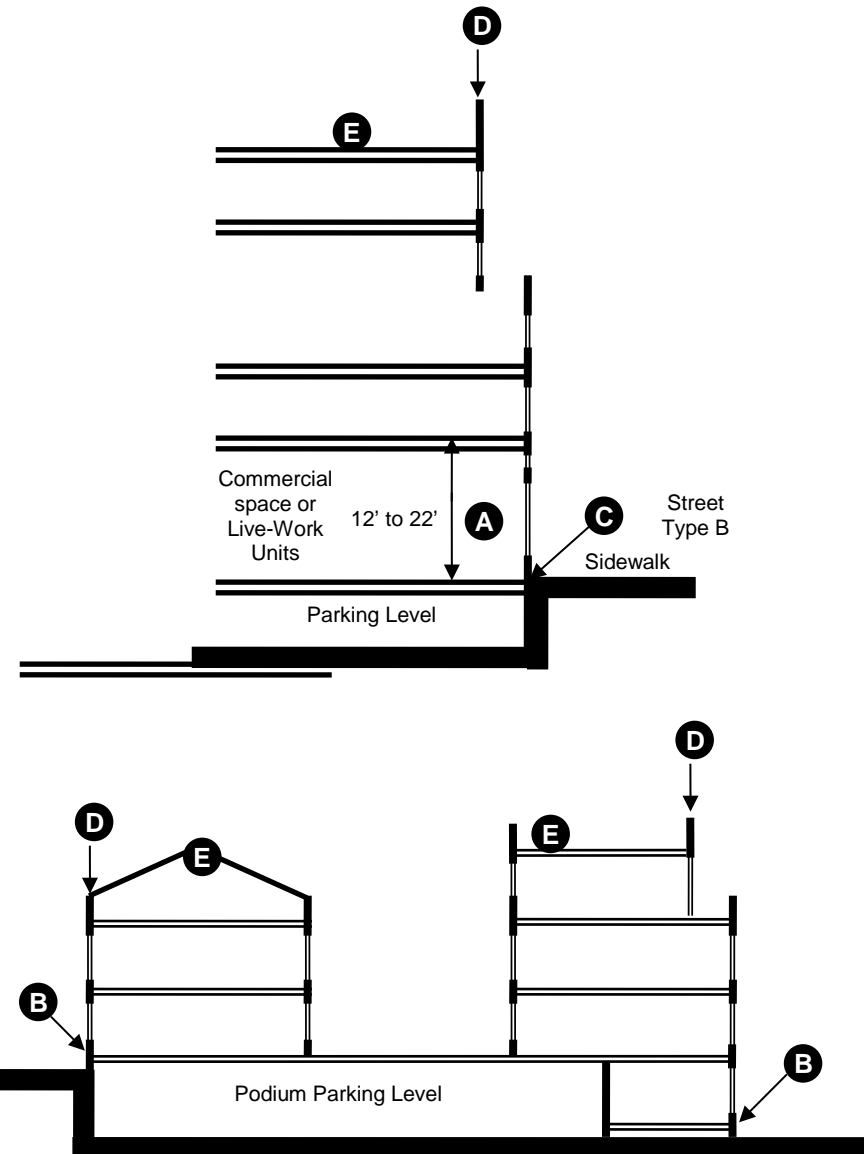
Roofs

PC HEARING DRAFT 04-28-2014

E

Roof Forms and Slope

A variety of roof forms are allowed to create diversity and interest.



Townhomes/Flats: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, a variety of frontages are allowed.

Facades along Street Type B shall be designed with one or more of the following building frontages:

Storefronts

Office fronts

Live-work fronts

Residential frontages

Stoops

Facades along all other street types shall be designed with one or more of the following building frontages:

Residential frontages

Stoops

Standards for the above frontages are provided in Section VI.7 (Frontages and Projections).

Townhomes/Flats: Open Space Requirements

At least ~~150-100~~ square feet of useable open space shall be provided for each unit. Useable open space includes stoops, balconies, roof-top gardens, courtyards, patio yards, playgrounds, and rear yards. ~~This open space requirement may be achieved by providing a combination of private spaces for each individual unit and/or providing common open space areas that serve multiple units. For a common open space to qualify, the space shall be at least 500 square feet with no dimension less than 10'. Also required in this area is 65 square feet per unit of private common usable space (common outdoor recreation or passive greenspace, tot lots, indoor fitness centers, etc.) and a total of 2.0 acres of public common open space west of Twin Oaks Valley Road.~~

Townhomes/Flats: Parking Requirements**Residential Units**

Minimum Requirement	1 space per unit
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Live Work Units

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
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Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
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Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

Townhomes/Flats: Building Uses**Upper Floors**

A	Permitted by Right	Residential
B	Conditionally Permitted Uses*	Not applicable

Ground Floor (with frontages along Street Type B)

C	Permitted by Right	Retail, General Service, Dining, Office, Residential, Live-Work
D	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Cultural/Civic Institutions (M), Alcohol Sales (dM)

Ground Floor (with frontage along all other street types)

E	Permitted by Right	Residential
F	Conditionally Permitted Uses*	Not applicable

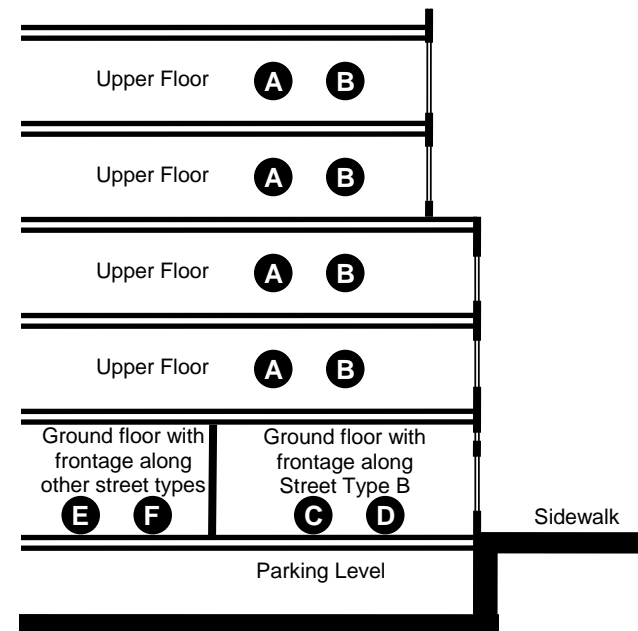
*** Notes:**

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.



Development Standards for Creek Side Townhomes/Flats

Description

Townhomes are multi-story residential units that are placed side-by-side and share side property lines with adjacent units.

Flats are residential units that are stacked vertically to create a multi-story building. Both townhomes and flats may be combined within one development. Building heights for townhouses and flats generally range from two to five stories.

Along Street Type B (Residential Avenue) and D-1 (One-Way Parkway), the ground floor of flats may contain commercial space and/or live-work units.

These Townhomes and Flats are located adjacent to the Creek Trail. Public access from the street to the Creek Trail is required.



Creek Side Townhomes/Flats: Lot Size and Building Placement

Block Subdivisions and Lot Size

The entire development site may be developed as one project or subdivided into smaller properties. If subdivided, a shared driveway system is required to provide access to the parking facilities of each property on the block.

Lot Width 16' minimum

Lot Depth 60' minimum

Building Setbacks from Property Lines (PL)

Front and Side (External and Internal Side) PL No setbacks are required

Rear PL 24' (private yards are prohibited in the rear setback)

A Build-to-Line (BTL) for Buildings with frontage along Street Types A-1 and B-and-D-1

Build-to-Line Building facades shall be placed within 0' to 8' of the property line along Street Type B.

B Exception A Where topography does not allow for BTLs within ranges noted above, the applicant shall confer with the Planning Director to decide upon the most appropriate design measures. Such measures should both maximize the site's functional connectivity to the rest of the project while mitigating excessive topographic elevation changes and grading cost constraints. Any deviations from the above BTL requirements are subject to a Director's Permit.

Frontage Buildout 90% to 100% of the site's total street frontage (the total length of front and external side property lines) shall be occupied by one or more building facades.

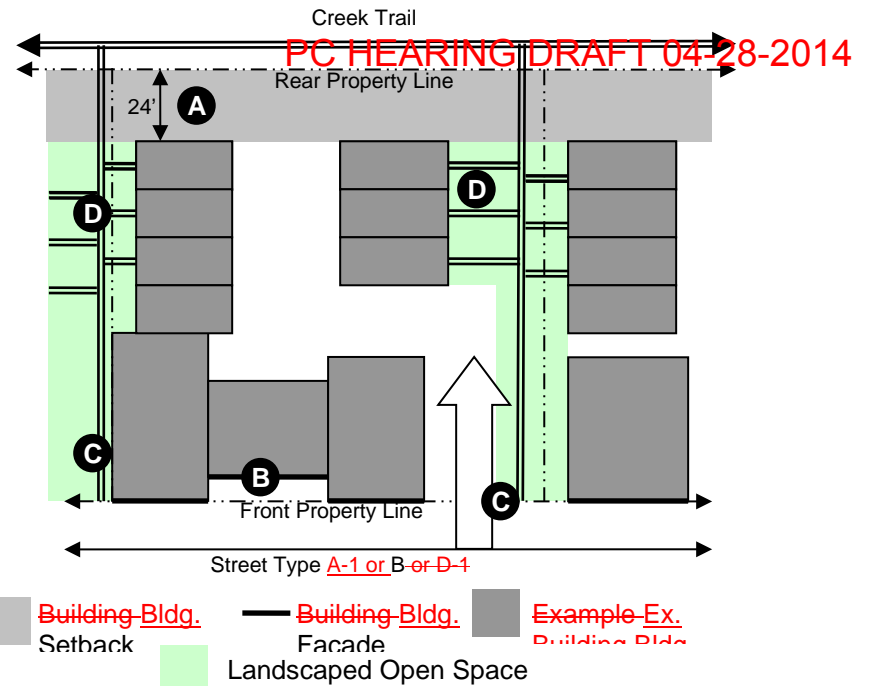
Exception A The frontage buildout requirement may be reduced by an additional 60' where driveway and pedestrian access to the Creek Trail are provided.

C Exception B The BTL may be set back to allow for forecourts.

Form Based Code: VI-112

Build to Line (BTL) for Buildings other Buildings

Build-to-Line There is no specific BTL for other buildings on the site. Buildings shall front landscaped



C Exception B The BTL may be set back to allow for forecourts.

Exception C Pursuant to BTL Exception A, any deviations from above frontage requirements are subject to Director's Permit.

Build to Line (BTL) for Buildings other Buildings

C Build-to-Line There is no specific BTL for other buildings on the site. Buildings shall front landscaped open spaces that connect the street to the Creek Trail.

Driveway Standards	
C	Driveways shall be designed to have a street-like character. Where feasible, they shall be aligned with street intersections, and accommodate two-way traffic. Both sides of the driveway shall have 5' wide parkways and 5' wide sidewalks. Driveways shall be paved with a material color and finish consistent with the adjoining street.

Creek Side Townhomes/Flats: Vehicle Access and Parking

Parking Options

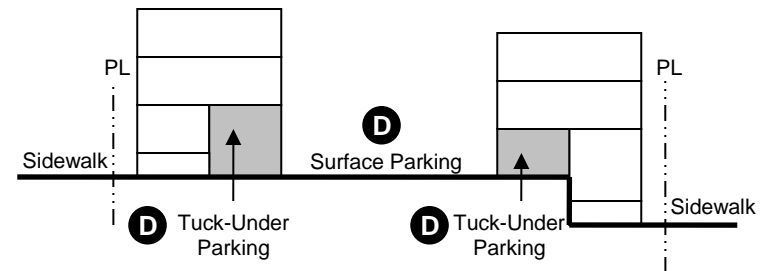
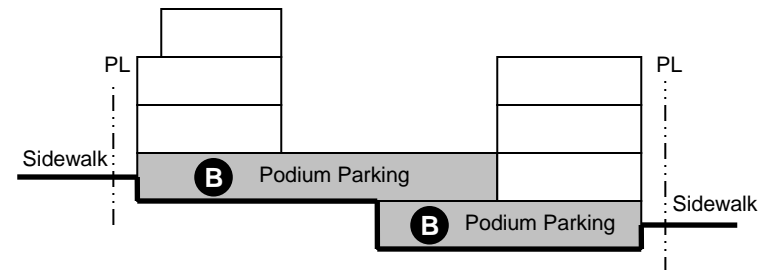
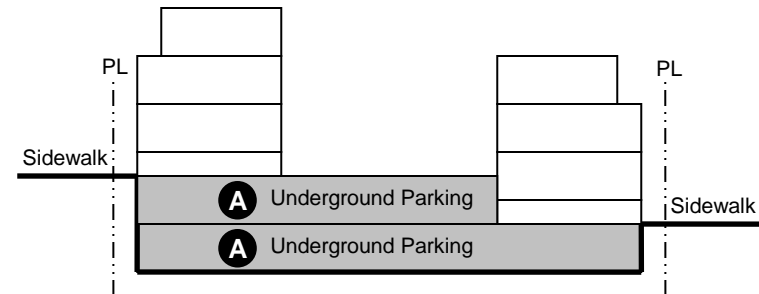
Combinations of parking options are allowed, including underground, podium, surface, and tuck-under parking. Shared parking facilities are also allowed to provide parking for multiple properties on the block.

Parking Locations

A Underground Parking Levels	Underground parking levels are allowed on the entire site.
B Podium Parking Levels	Podium parking levels are allowed within all areas of the site, excluding setbacks.
C Surface and Tuck-Under Parking	Surface parking lots and tuck-under parking facilities are allowed if they are screened from public view by locating them to the rear of the townhomes/flats on the block.
D	Access to tuck-under parking is only allowed along the rear facade.

Driveways

Allowed and Prohibited Locations	Driveways are only allowed to connect to Street Types <u>A-1 and B, D-1, and E-1</u> . Along Street Type B, the driveways shall connect to a street intersection. If the block is subdivided into multiple lots, a shared driveway system shall be created to provide access to the parking facilities for the each lot. Driveway access point should be limited to 5 per block to reduce the number of curb-cuts and pedestrian/vehicle conflicts along the street.
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Creek Side Townhomes/Flats: Building Heights and Mass	
Number of Floors	
Minimum	See Figure VI.C
Maximum	See Figure VI.C
Floor-to-Floor Heights (excluding parking levels)	
Ground Floor	Along Street Types <u>A-1 and B-and-D-1</u> , the ground floor of flats may contain commercial space and/or live-work units. If the building is designed with commercial space or live-work units on the ground floor, the floor-to-floor height of the ground floor shall be 12' to 22'.
Ground Floor	
Elevation	The elevation of the ground floor shall be within 0" to 48" above the grade of the adjacent sidewalk.
Exception <u>A</u>	The elevation of the ground floor where residential uses are located shall be within 18" to 48" above the average grade of the adjacent sidewalk or paseo. This standard only applies to buildings with a frontage along a street.
Stepbacks	
Allowed and Prohibited <u>Exception B</u>	<u>Where topography does not allow for ground floor elevations within ranges noted above, applicant shall confer with Planning Director to decide upon most appropriate design measures. Such measures should both maximize site's functional connectivity to rest of project while mitigating excessive topographic elevation changes and grading cost constraints. Any deviations from the above ground floor elevation requirements</u>

	are subject to a Director's Permit. Stepbacks are allowed on the third floor and above, but are not required.
Roofs	
A Roof Forms and Slope	A variety of roof forms are allowed to create diversity and interest.

Stepbacks

D Allowed and Prohibited

Stepbacks are allowed on the third floor and above, but are not required.

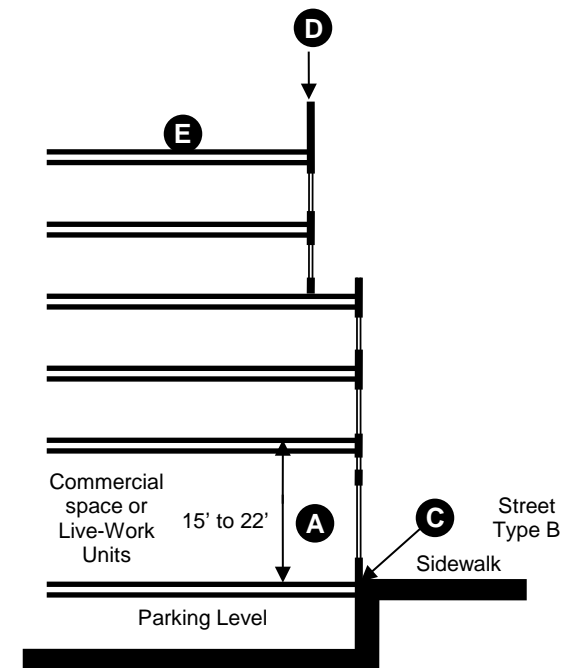
Roofs

E of Forms and Slope

A variety of roof forms are allowed to create diversity and interest.

B

C



Creek Side Townhomes/Flats: Frontages

All facades that are adjacent to a street or public space shall be designed with a specific building frontage. As indicated below, a variety of frontages are allowed.

Facades along Street Type ~~A-1 and B~~ and ~~D-1~~ shall be designed with one or more of the following building frontages:

Storefronts

Office fronts

Live-work fronts

Residential frontages

Stoops

Facades in other locations shall be designed with one or more of the following building frontages:

Residential frontages

Stoops

Standards for the above frontages are provided in Section VI.7 (Frontages and Projections).

Creek Side Townhomes/Flats: Open Space Requirements

At least ~~150-100~~ square feet of useable open space shall be provided for each unit. Useable open space includes stoops, balconies, roof-top gardens, courtyards, patio yards, playgrounds, and rear yards. Also required in this area is 65 square feet per unit of private common usable space (common outdoor recreation or passive greenspace, tot lots, indoor fitness centers, etc.) and a total of 2.0 acres of public common open space west of Twin Oaks Valley Road. This open space requirement may be achieved by providing a combination of

private spaces for each individual unit and providing common open space areas that serve multiple units. For a common open space to qualify, the space shall be at least 500 square feet with no dimension less than 10'.

Creek Side Townhomes/Flats: Parking Requirements

Residential Units

Minimum Requirement	1 space per unit
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Live Work Units

Minimum Requirement	1 space per unit if the unit is less than 2,000 square feet 2 spaces per unit if the unit is greater than or equal to 2,000 square feet
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Non-Residential Uses

Minimum Requirement	2 spaces per 1,000 square feet of leasable space
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Allowed Locations

Parking for all uses may be provided on-site, on-street adjacent to the site, or within an off-site shared and/or public parking facility.

Transportation Demand Management (TDM)

Refer to Chapter V – Transportation | Circulation for more information on parking requirements related to TDM measures

|

Creek Side Townhomes/Flats: Building Uses**Upper Floors**

A	Permitted by Right	Residential
B	Conditionally Permitted Uses*	Not Applicable

Ground Floor (with frontages along Street Types A-1 and B-and-D-1)

C	Permitted by Right	Retail, General Service, Dining, Office, Residential, Live-Work
D	Conditionally Permitted Uses*	Outdoor Dining (d), Dining/Entertainment (d), Cultural/Civic Institutions (M), Alcohol Sales (<u>dM</u>)

Ground Floor (with frontage along all other street types)

E	Permitted by Right	Residential
F	Conditionally Permitted Uses*	Not Applicable

*** Notes:**

(d) Use requires Director's Permit

(m) Use requires Minor Use Permit

(M) Use requires Major Use Permit

Definitions for the above uses, including types of businesses that are specifically prohibited, are provided in Section VI.11 (Definitions). Refer to Chapter IX (Implementation and Administration) for uses not specifically listed.

